

ORDINANCE NO. 2016-48

APPROVING A CONCEPT DEVELOPMENT PLAN FOR 108
ACRES INCLUDING PART OF THE FORMER BLUE ASH
AIRPORT AND AN ADJACENT PARCEL IN AN M-3 PLANNED
OFFICE AND MIXED USE DISTRICT

WHEREAS, Al. Neyer, LLC has submitted development plans per the requirements of Chapters 1165 and 1185 to create a mixed-use Planned Development on approximately 108 acres adjacent to Summit Park within an M-3 Planned Office and Mixed Use District; and

WHEREAS, the proposed plans include a development pattern and proposed land uses consistent with the requirements of the Zoning Code and consistent with the 2015 City of Blue Ash Land Use Plan; and

WHEREAS, the proposed Planned Development is described on materials submitted by the applicant including a 40-page booklet entitled, "Summit Park Land Development," six civil site plan pages by Cardinal Engineering, two pages of apartment building elevation drawings by pdt architects, two pages of office building elevation drawings by Al. Neyer, and 16-pages of elevation drawings and schematics describing the proposed single-family residential area bearing the notation "Ordinance No. 2016-48, September 22, 2016"; and

WHEREAS, the City of Blue Ash Planning Commission recommended approval of a Concept Development Plan for the proposed development on August 4, 2016, as reflected in its meeting minutes, in accordance with Chapter 1185 of the Planning and Zoning Code; and

WHEREAS, notice was provided to certain property owners as required in Chapter 1185; and

WHEREAS, rights to develop property in the M-3 District are subject to City Council legislatively approving a Final Development Plan for the property by Ordinance; and

WHEREAS, a public hearing was held before the Council of the City of Blue Ash, Ohio, on September 8, 2016.

Be it ordained by the Council of the City of Blue Ash, Ohio,

SECTION I.

The proposed Concept Development Plan on approximately 108 acres adjacent to Summit Park in an M-3 Planned Office and Mixed Use District is hereby approved in accordance with Chapters 1165 and 1185 of the Planning and Zoning Code of the Codified Ordinances of the City of Blue Ash, Ohio. Details of the Concept Development Plan are described on materials submitted by the applicant including a 40-page booklet entitled, "Summit Park Land Development," six civil site plan pages by Cardinal Engineering, two pages of apartment building elevation drawings by pdt architects, two pages of office building elevation drawings by Al. Neyer, and 16-pages of elevation drawings and schematics describing the proposed single-family residential area bearing the notation "Ordinance No. 2016-48, September 22, 2016" and specifically including all conditions, provisions, and restrictions as set forth in and on said materials, in the minutes of the Planning Commission dated August 4, 2016, and specifically subject to the following:

1. Approval of the concept plan does not guarantee approval of the final development plan.
2. Final plan approvals based upon this approved Concept Plan shall be reviewed per the criteria in Section 1183.05 in addition to consistency with the approved Concept Plan. Final plan consideration will include evaluation of details sufficient to ensure that Planning Commission and City Council understand the physical

development pattern, the character of future public lands, the architectural design character of proposed buildings, the general land use categories for which each proposed building is designed, and the likely traffic impact on adjacent and nearby rights-of-way.

3. The development is intended to be a vibrant employment district that includes supportive residential and commercial amenities. In its deliberation of all Final Plans based upon this Concept Plan, Council shall consider the nature and mix of existing land uses that have been developed to date to ensure that continued development maintains the appropriate and balanced mix of land uses. Final plan approvals that contain residential land uses may be withheld or delayed to ensure that the actual developed conditions reflect this desired development character. For purposes of this approval, institutional residential uses such as retirement or senior living facilities or medical rehabilitation facilities may be considered a residential land use.
4. The density and mix of uses within the development shall be subject to the availability of city facilities, resources, and infrastructure networks.
5. Nothing in this Concept Plan approval shall preclude future zoning district or text amendments by City Council.
6. Final Plan approvals shall be subject to the zoning code in effect at the time of plan submittal.

SECTION II.

It is hereby determined that the proposed Planned Development will not be detrimental to the public peace, health, safety or general welfare, and that it is in the best interest of the City of Blue Ash, Ohio.

SECTION III.

This ordinance shall take effect and be in force from and after the earliest period provided by law.

PASSED this 22nd day of September 2016.

Lee Czerwonka, Mayor

Jamie K. Eifert, Clerk of Council

APPROVED AS TO FORM:

Bryan E. Pacheco, Solicitor

SUMMIT PARK Land Development

Planned District Development Plan
July 29, 2016

ORDINANCE No. 2016-48
September 22, 2016

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Executive Summary

Competitive Context

While the Blue Ash office submarket has historically maintained Greater Cincinnati's second largest Class A office concentration, recent trends have shown slippage in comparable rents and absorption of new space. Over the past decade only 100,000 square feet of Class A office space has been developed within Blue Ash while nearly 2,000,000 square feet of Class A space has been developed along the I-71 corridor.

The revitalization of Over-the-Rhine and the development of walkable mixed-use office nodes at the Cincinnati Banks, Rookwood Exchange, Kenwood Collection and Liberty Center have drawn employers seeking to offer their employees opportunities to live, shop and play within a walkable radius of their place of employment. In fact, recent studies analyzing the largest 30 markets in the US show that suburban office parks across the country are losing tenants to city centers and walkable suburban mixed-use districts.

The Opportunity

The redevelopment of the former Blue Ash Airport site provides an unparalleled opportunity to re-establish Blue Ash and the Summit Park District as one of the region's premier corporate addresses. The Vision for the redevelopment of the Airport Property draws from successful examples of regional employment destinations integrated with and around walkable mixed-use districts in suburban communities such as those in Grandview and Dublin, Ohio; the Mid-Atlantic region and a host of comparable districts throughout the country.

Guiding Principles of the Master Plan

The master plan for the Summit Park land development employs a set of guiding principles that have influenced the design of the development's initial entitlements and will guide subsequent phases of development as the district grows and develops over the next 10 years. These principals are summarized below and are expressed within the visioning and design standards which are depicted in the following Master Plan Section.

1. Leverage the Value of the Park - Integrate the open space network of the park with the natural systems, greenways and civic spaces of the former airport property and extend the open space network to the south and east along the headwater streams of Sycamore Creek.

2. Create a Diverse Mix of Uses with Shared Infrastructure - Provide a mix of retail, dining and residential uses which appeal to the current and future workforces of targeted corporate tenants AND provide current Blue Ash residents choices for new housing which meets their evolving lifestyle preferences and needs. Mixed use districts make more efficient use of shared development infrastructure and lessen vehicular trip generation during peak morning and afternoon drive times.

3. Provide a Walkable/Bikeable Street Network - Incorporate Bike lanes and multi-purpose paths that link the park and land development to the city's larger path network.

4. Provide Multiple Ways to Get Around - Work with public transit authorities and private services such as "Uber" and "Lyft" to establish transportation options which appeal to employees and residents seeking alternatives to the cost, risk and environmental impacts of automobile ownership.

5. Establish a Sufficient Critical Mass of Residents, Amenities and Services to Create a Vibrant, Mixed-use District - Vibrant Mixed-use districts contribute to increased property values both within the district and in established adjoining neighborhoods.

6. Connect to Surrounding Development Areas and the City - Establish additional roadway connections which link the Summit Park District to the east side of Reed Hartman Highway, Downtown Blue Ash and Plainfield Road.



AERIAL VIEW - Proposed Summit Park Land Development

Executive Summary



Section 1 WELCOME TO SUMMIT PARK

About Blue Ash

The Summit Park District is located just 20 miles north of Downtown Cincinnati in Blue Ash, Ohio. Blue Ash offers a unique blend of small-town charm, top-rated amenities, major employers, and big city access that makes it the perfect place to advance your career, have fun, and enjoy the good life.

Amenities - The real estate website, Movoto.com, recently ranked Blue Ash as the third top city in Ohio for amenities and it's easy to see why. There's so much to do here! A long list of great restaurants, free summer concerts, annual festivals that attract more than 300,000 people, and the State-of-the-art Blue Ash Recreation Center are just a few.

In addition, there's the Blue Ash Golf Course, which is ranked as one of the top 75 municipal courses in the country by Golf Digest magazine, and several beautiful parks, including our newest gem, Summit Park. Spanning 130-acres, the park will have a performance pavilion, restaurants, an observation tower, ice skating rink, and many other recreational facilities. And, if you need the ideal place to host your next corporate or social event, the stunning Cooper Creek Event Center is it.

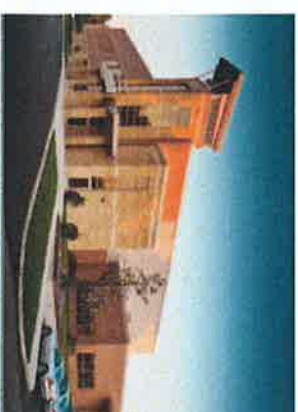
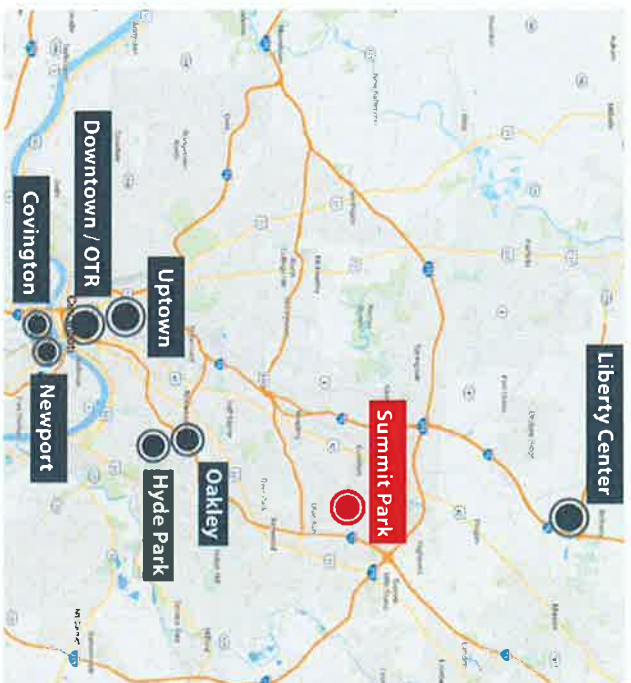
Exceptional Schools - Sycamore Community Schools serves the majority of Blue Ash and its high school is ranked as one of the top in the state of Ohio by U.S. News and World Report. Blue Ash is also home to several parochial schools, including Ursuline Academy, an exclusive four-year college-preparatory/Catholic high school for girls. If you're sending your kids to off to college or are considering taking some courses or

getting a degree, the University of Cincinnati Blue Ash College is conveniently located on a 135-acre suburban campus and is the University of Cincinnati's third largest college by enrollment.

Excellent Career Opportunities - If you're looking for job opportunities, you won't have to look far from home. Blue Ash is the largest suburban office market in Greater Cincinnati. More than 2,300 of the most successful companies in the region and around the globe are located here, including Fortune® 500 firms and the corporate headquarters of several leading companies.

Convenient Big City Access - Within a 20-minute drive, you can be nearly anywhere in Greater Cincinnati, including downtown Cincinnati's many cultural, entertainment and sporting venues, like the Great American Ballpark, the home of the Reds, and Paul Brown Stadium, the home of the Bengals. In addition, if you work in another community, your commute will take far less time than from many other areas in the region.

Source: <http://www.blueash.com/residents/index.php>



BLUE ASH RECREATION CENTER

About the Park

About

Summit Park is a 130 acre, \$73,000,000 city park located within the geographic center of Blue Ash and the Summit Park District.

The park is currently under its 3rd phase of construction and, when completed, will feature over four miles of multi-purpose trails, an 8,000 SF community pavilion, 12,000 SF covered event space, and several cafés and bistros organized along the axis of an expansive civic commons and 2.5 acre performance lawn and stage. The latest amenities include an observatory tower, rainwater harvesting pond, and the development of approximately 20 acres of native meadow and woodlands.

Summit Park has already become a regional destination for musical performance and is expected to become a significant contributor to the appeal of the proposed project and an anchor for economic development throughout the City's Summit Park District.

http://www.blueash.com/departments/parks_and_recreation/summit_park/index.php



SUMMIT PARK MASTER PLAN



AERIAL PERSPECTIVE VIEW OF PARK COMMONS

Goals & Design Principles for the Proposed New Development

Competitive Context

The revitalization of Over-the-Rhine and the development of walkable mixed-use office nodes at the Cincinnati Banks, Rookwood Exchange, Kenwood Collection and Liberty Center have drawn employers seeking to offer their employees opportunities to live, shop and play within a walkable radius of their place of employment. In fact, recent studies completed by Chris Lemberger and the Brookings Institute show that suburban office parks across the country are losing tenants to city centers and walkable suburban mixed-use districts.

Goals

The redevelopment of the former Blue Ash Airport site provides an unparalleled opportunity to re-establish Blue Ash and the Summit Park District as one of the region's premier corporate addresses.

In order to leverage the Summit Park development as a catalyst for increased investment and improved tenancy throughout the district we believe that the following principals should be employed:

Design Principles

1. **Leverage the Value of the Park** - Integrate the open space network of the park with the natural systems, greenways and civic spaces of the former airport property and extend the open space network to the south and east along the headwater streams of Sycamore Creek.
2. **Create a Diverse Mix of Uses with Shared Infrastructure** - Provide a mix of retail, dining and residential uses which appeal to the current and future work forces of targeted corporate tenants AND provide current Blue Ash residents choices for new housing which meets their evolving lifestyle preferences and needs. Mixed use districts make more efficient use of shared development infrastructure and lessen vehicular trip generation during peak morning and afternoon drive times.
3. **Provide a Walkable/Bikeable Street Network** - Incorporate bike lanes and multi-purpose paths that link the park and land development to the city's larger path network.
4. **Provide Multiple Ways to Get Around** - Work with public transit authorities and private services such as "Uber" and "Lyft" to establish transportation options which appeal to employees and residents seeking alternatives to the cost, risk and environmental impacts of automobile ownership.
5. **Establish a Sufficient Critical Mass of Residents, Amenities and Services to Create a Vibrant, Mixed-use District** - Vibrant Mixed-use districts contribute to increased property values both within the district and in established adjoining neighborhoods.
6. **Connect to Surrounding Development Areas and the City** - Establish additional roadway connections which link the Summit Park District to the east side of Reed Hartman Highway, Downtown Blue Ash and Plainfield Road.

LEVERAGE VALUE OF PARK



"CRITICAL MASS"



MIXED-USE AND SHARED INFRASTRUCTURE



WALKABLE / BIKEABLE



MULTIPLE WAYS TO GET AROUND TOWN



CONNECT TO SURROUNDING DEVELOPMENT & CITY



Planning and Zoning



Zoning

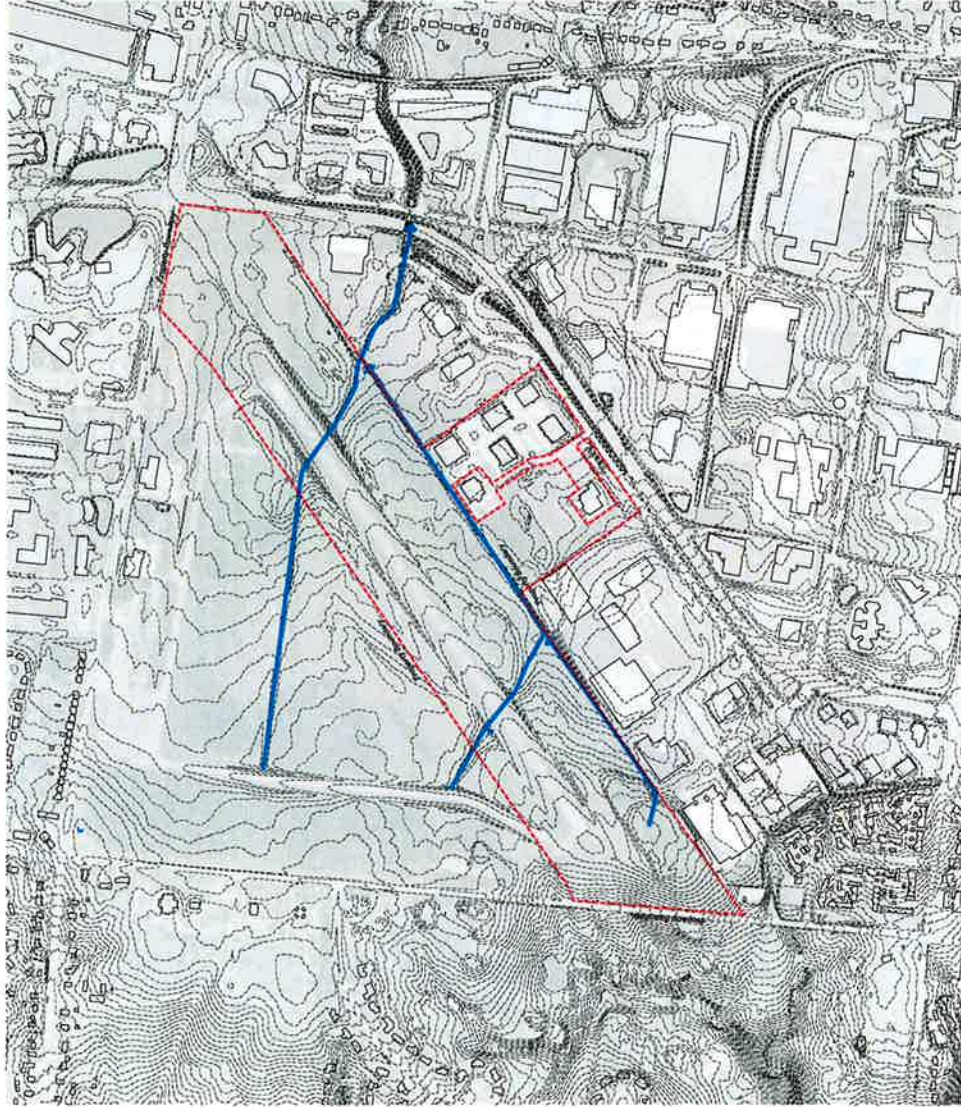
The parcels are currently zoned M1 Office – Industrial District. The proposed plan requires a change in zoning from M1 to M3: Planned Office and Mixed Use District. The current M3 zoning text requires an amendment to sufficiently account for the range of development envisioned for the site. The developer will be submitting a proposed zoning text amendment to the City of Blue Ash under a separate cover to account for the necessary changes.

In addition to the M3 zoning text amendment, a modification to the Planned District ordinance will be necessary to approve a Concept level plan as part of the zoning change. The proposed modification to the Planned District ordinance will be submitted by the developer to the City of Blue Ash under a separate cover. The change will create an optional Concept Plan submital that will allow Planning Commission and City Council to review and approve the broad concept for the entirety of the site, including range of uses (as permitted by M3 zoning), the approximate location of primary street corridors, public space concepts, conceptual stormwater and grading, and a project narrative. This will be used as a guide for phase by phase site plan submitals to ensure compliance.

Each phase of development will submit to Planning Commission and City Council under the Planned District (Section 1185) site plans that describe in greater detail the form of the development, the developed square footage, public space concepts, parking, servicing/ access, etc. Final subdivision, plans and approvals for each site will be approved subsequent to the approval of the Planned Districts.

Section 2 EXISTING CONDITIONS

Hydrology



Hydrology, Stormwater and Drainage

The former airport site is comprised of three primary watershed areas. The westerly end of the site containing approximately 11 acres is moderately sloped with existing grades generally between 2 and 10 percent and draining generally west toward Plainfield Road. The area immediately east of the previously described area containing approximately 26 acres drains generally to a small stream which crosses the site flowing north to south terminating at a man-made, concrete-lined channel located along the southerly property line and flowing from the southwest to the northeast. An approximately 260-foot portion of the stream was previously enclosed in a corrugated metal culvert pipe to facilitate construction of the airport runway. This portion of the airport runway existing grades generally ranging between 1 and 5 percent. The balance of the airport site containing approximately 61 acres drains generally to a small stream (including areas intercepted by the man-made, concrete-lined channel located along the southerly property line) which crosses the site flowing northwest to southeast. An approximately 320-foot portion of the stream was previously enclosed in twin corrugated metal culvert pipes to facilitate construction of the airport runway. This stream intercepts the man-made, concrete-lined channel at the airport property's southerly boundary, continues to flow easterly beyond the project boundaries. This portion of the site gently sloped with existing grades generally ranging between 1 and 5 percent.

The former senior center property, contain approximately 11 acres in total, is gently sloping and drains general from north to south. The site's runoff is currently collected into a private on-site storm sewer system which empties into a stormwater runoff area located in the property's southeast corner. The out fall from this basin is directed to an existing concrete-lined channel to the south located between Carver Road and Reed Hartman Highway. The site lies in an area generally served by a separated storm sewer system managed by the City of Blue Ash Public Works Department. Stormwater management practices with the development will be incorporated to meet or exceed the requirements of the City of Blue Ash and the Hamilton County Storm Water District. It is the developer's intent that stormwater management practices utilized in the development to address both stormwater quantity and quality reflect current best management practices, and encourage high levels of innovation, creativity, environmental effectiveness and cost effectiveness. These may, including the use of methods that allow for groundwater recharge, that incorporate sustainable development practices, and incorporate measures that utilize vegetation and soil media to filter, treat or infiltrate stormwater where suited to site and development conditions. Stormwater features may be integrated into open space areas designed as formal or natural amenities. Stormwater management features shall be designed by a licensed design professional.

LEGEND

Property Boundary



Jurisdictional Streams

Tree Survey



- LEGEND**
- Woodland
 - Woodland Scrub
 - Meadow
 - Fescue Lawn

Vegetation across the former airport site has been greatly influenced by the cultural maintenance practices of the airport's operation. Primary and secondary growth trees and associated shrub vegetation have established along some of the steeper slopes along the southern property boundary outside of the clear zones required for flight operations. Native grasses and forbs have begun to establish within the mixed turfgrass areas that were maintained when the airport was operational.

Tree species evident on the site are similar to those on the west side of the park and include Maple, Ash, Sycamore, Eastern Red Cedar, and Box Elder among other pioneer species.

Understory shrubs are dominated by Honeysuckle but include Dogwood and Viburnum.

Grading







Grading

The natural site topography has been largely disturbed by the excavations and embankments performed to construct the runway which runs much of the length of the property. It is anticipated that the proposed redevelopment will require grading across significant portions of the project site to create building lots, pads and parking areas as well as for construction of roadway infrastructure and pedestrian corridors. It is anticipated that cuts and fills will generally not be in excess of 15 feet in depth and may be performed using conventional earthwork equipment and techniques.

Soil Conditions

Preliminary subsurface investigations and geotechnical analyses have been performed by Alt & Witzig Engineering, Inc. including six (6) soil borings and twelve (12) test pit excavations to study the subsurface conditions as they relate to development and construction of the proposed mixed-use development. The soil borings were performed to a maximum depth of 20 feet or terminated in bedrock. Test pit excavations were performed to a maximum depth of 10 feet or terminated in bedrock. Soil conditions varied generally across the site. At the immediate surface, borings and excavations encountered approximately 3 to 11 inches of topsoil. Beneath the surface materials, brown and gray clay underlain by brown and gray silty sandy clay and weathered shale and limestone was encountered to bedrock. These soils are generally well suited as bearing material to support the proposed new construction. Areas of shallow bedrock were observed.

LEGEND

-  0-2% Slope
-  2-5% Slope
-  5-15% Slope
-  15%+ Slope

with shale and limestone becoming hard at depths ranging from 3 to 10 feet below existing grade located in the north central portion of the former airport site. Materials in these areas may require additional excavation effort and moisture conditioning if used for structural fill. Areas of soft, wet soils were observed in the northeast portion of the former airport site, which may require modification if used for bearing or as structural fill. Lenses of fat clay soils, that can be susceptible to moderate to significant shrinking and swelling with moisture variation, were visually identified in three (3) of the test pit excavations and will require further investigation prior to construction. Additional geotechnical subsurface exploration and analyses will be completed prior to new construction.

Sanitary Sewer

An existing Metropolitan Sewer District of Greater Cincinnati (MSD) 8-inch diameter sanitary sewer main runs along the southerly boundary of the former airport property. The depth of the existing 8-inch sanitary sewer varies between approximately 8 and 14 feet across the development site. The 8-inch main drains from southwest to northeast beginning at a manhole located approximately 270 feet east of the site's westerly boundary at Plainfield Road and exiting the property at a point approximately 1,100 feet west of the site's easterly boundary at Reed Hartman Highway. The main continues along the existing stream corridor east to a manhole on the east side of Reed Hartman Highway, at which point the sewer becomes an 18-inch diameter sanitary sewer trunk line. Based on meetings and correspondence with the MSD, sewerage to the site to drain easterly (consistent with the existing sewer main on the property) will avoid impacts to downstream Sanitary (SSO) and Combined Sewer Overflow (CSO) issues. MSD has commented that the capacity in the downstream 18-inch sanitary sewer line is anticipated to have sufficient available capacity to serve the proposed development. MSD is currently modelling the subject sewers to confirm capacities and to serve as a basis for determining thresholds and timing for potential improvements. It is anticipated that construction of new on-site public sanitary sewer main extensions will be required on site to serve the proposed development, and those main extensions will be directed to the existing downstream 18-inch trunk sewer. It is further anticipated that the project sewer demand at some point in the future will require construction, through existing sewer easements, of a larger or parallel sewer main from the project's southern boundary to the existing 18-inch sewer east of Reed Hartman Highway.

Water

The subject site is served by Greater Cincinnati Water Works mains at the site's perimeter including:

- 12-inch diameter water main along Glendale Milford Road along the site's northernmost boundary
- 16-inch diameter water main along Reed Hartman Highway at the site's easternmost boundary
- 12-inch diameter water main along Carver Road at the site's southernmost boundary
- 12-inch diameter water main along Plainfield Road at the site's westernmost boundary

Preliminary information provided by Greater Cincinnati Water Works is that water is available in sufficient capacity to serve the proposed development. It is anticipated that construction of new on-site public water mains will be required to serve the proposed development and that any such mains will be connected to the existing public water network at the property's boundaries.

Electric and natural gas

The site is served with electric and natural gas by Duke Energy with a number of existing mains and distribution lines available at the property perimeter to be accessed for extension of new and redundant service runs to support redevelopment of the property.

Section 3 THE CONCEPT PLAN

Context & Access



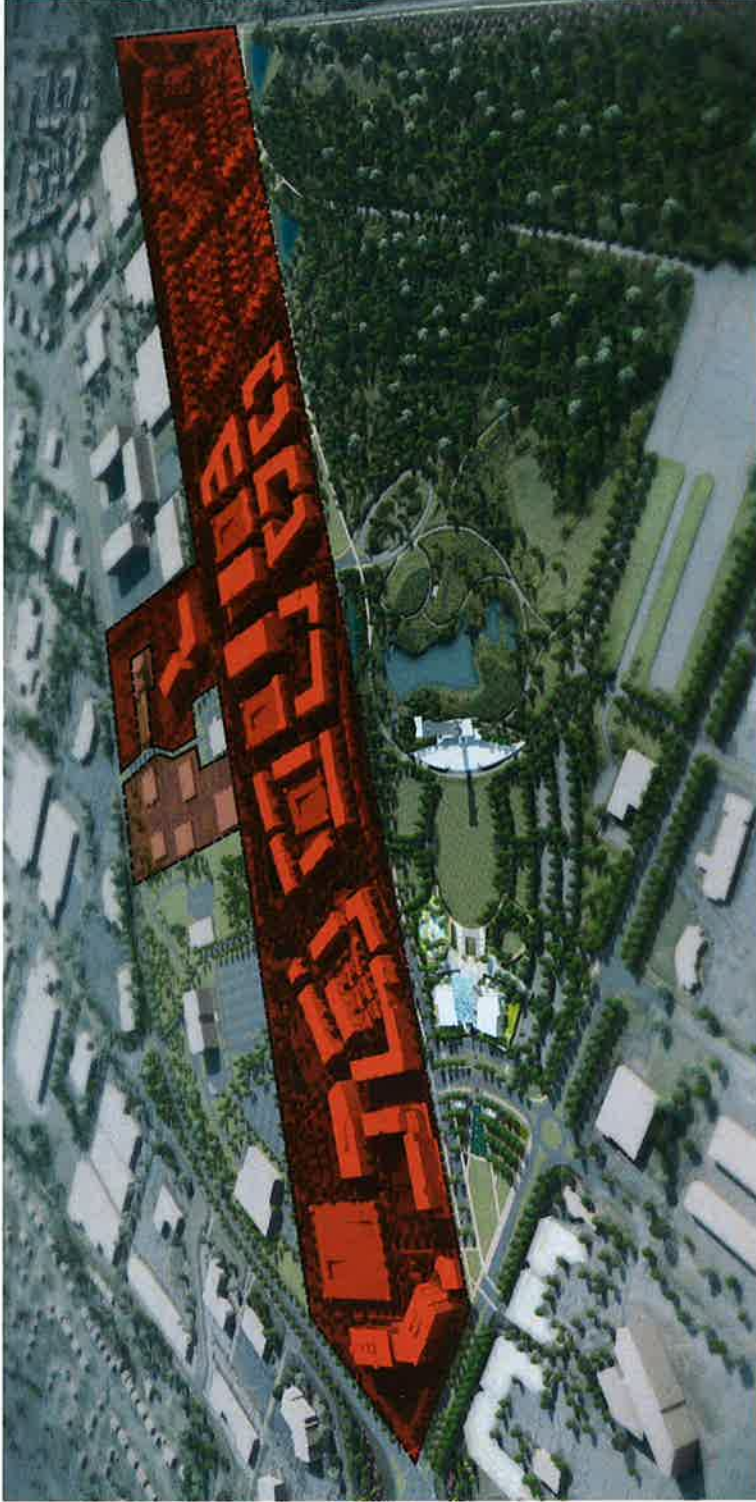
The Proposed Summit Park Land Development is located at the crossroads of Reed Hartman Highway and Glendale-Milford Road on the site of the former Blue Ash Airport.

The site is served with exceptional regional connectivity and the City of Blue Ash is currently evaluating alternatives for improvements along Glendale-Milford Road and Reed Hartman Highway aimed at improving peak-hour traffic flows to and from Interstates 71 & 75, the Ronald Reagan Highway and Interstate 275.

Access onto the property is currently limited to the existing Carver Woods Drive and the new Summit Place connection at the northeast entrance to the park.



Development Area



The gross developable area is approximately 108 acres.

LEGEND

- EXISTING ZONING TO REMAIN
- AREA TO BE REZONED

The Vision



Summit Park Land Development

The Summit Park land development will be a signature, walkable, mixed-use development consistent with the City of Blue Ash's vision as laid out in its Land Use Plan for the Summit Park District. The project will leverage Blue Ash's \$75 million investment in Summit Park, serve as a catalyst for development across the entire Summit Park District and become a regional draw for businesses and investment.

The master plan for the development is built upon the fundamental principles of highly successful, walkable, mixed-use districts and establishes a framework for the current and future integration of Class A office spaces, a diverse mix of high quality rental and owner-occupied housing, specialty retail and dining venues, and a variety of entertainment, fitness and leisure options.

The plan establishes four distinct but closely integrated neighborhoods. Streets and open spaces are designed to create a vibrant village atmosphere and high-quality sense of "place" through the thoughtful integration of activity nodes. The development will contain uses that complement and build upon the energy of the Park and provide a place for people to gather, shop, eat, work and live. The development will include exceptional pedestrian connectivity to the park and adjoining neighborhoods and offer a thoughtful hierarchy of well landscaped and amenitized public spaces. Buildings and site amenities will adhere to high standards of design and construction and enhance Blue Ash's reputation as one of the region's most desirable places to work, live and visit.

Places in the Plan



The site naturally segments itself into four distinct neighborhoods that can develop over time, linked to each other and to Summit Park via contiguous public spaces, streets, and trails. The **Gateway Village** is a vibrant, mixed-use center with a strong employment focus. The **Parkview Neighborhood** is envisioned to be a mix of residential, office, senior living, and supportive retail. **Carver Woods** presents an opportunity to infill, intensify, and refresh employment uses that have historically lined Reed Hartman Highway. It also creates the potential for a visible Summit Park entrance from Reed Hartman Highway, better integrating the Summit Park into the heart of Blue Ash and better connects Blue Ash to Summit Park. **Golf View** is a traditional residential neighborhood development that offers timeless residential living modeled after the region's best neighborhoods.

Gateway Village [±30 Acres]

Summit Park's Gateway District will be a destination walkable mixed-use environment located adjacent to the most active areas of Summit Park. It is intended to include:

- Corporate headquarters location and/or multi-tenant 200,000 – 400,000 square feet of Class A office between three and seven stories in height
- Multi-family apartments: 220 – 350 units of high-end rental apartments in three to four story corridor-loaded buildings including a portion located over ground floor retail;
- Retail/restaurant: 25,000 – 60,000 square feet of specialty retail, service, restaurant, bar and entertainment space, including approximately 20,000 – 40,000 square feet of ground floor under multi-family;
- Hospitality: one to two 100 – 150 room nationally branded limited service hotels
- Possible grocery, indoor/outdoor music and performance venue



Gateway Village

Image and Character

The form of the Gateway District will largely be up to seven stories with primary frontages along streets and parks activated by retail, lobbies and other building entrances. On-street convenience parking will help buffer the sidewalks from the street. Much of the off-street parking will be screened by buildings that separate the parking from the street.



ARENA DISTRICT - Columbus, Ohio



GRANDVIEW YARD - Columbus, Ohio



SOUTH LAKE UNION - Seattle, Washington



GATEWAY UNIVERSITY DISTRICT - Columbus, Ohio



U-SQUARE - Cincinnati, Ohio



GLENDALE GALLERIA - Glendale, California



BROOKLAND - Washington, D.C.

Parkview [±35 Acres]

Parkview will be a walkable, mixed-use neighborhood that integrates a wide variety of commercial and residential uses adjacent to the natural beauty and abundant green space of Summit Park. It is envisioned to include:

- Class A office: 100,000 – 300,000 square feet of Class A office up to seven stories in height aligning Carver Woods Parkway
- Residential: Dwellings containing a variety of residential products with the mix driven by achieving desired neighborhood characteristics and accommodating market demand, including both for-sale and rental multi-family, condominiums, town homes, and senior living
- Commercial Services, Hospitality, and Healthcare Facilities.
- Retail/restaurant: Neighborhood retail, restaurant and bars that may include both freestanding and ground floor space vertically integrated with other uses



Parkview

Image and Character

Parkview is shaped by the greenways that bookend it, the prominent Summit Park frontage, and the access to Reed Hartman via Carver Woods. It's form is predominantly 2-7 stories with a variety of street frontage types based on the building's use or uses. Parkview is the largest of the mixed-use neighborhoods by area and, therefore, will have many special places within it that are differentiated by their public space programming, the scale of development, and approach to architecture.



ORENCO STATION - Hillsboro, Oregon



GARDEN PARK - Daybreak, Utah (Senior Living)



CELEBRATION AVENUE - Celebration, Florida



U-SQUARE - Cincinnati, Ohio



HORIZON BAY - Tampa (Senior Living)



SOLANA WILLISTOWN - Pennsylvania (Senior Living)



CELEBRATION - Florida

Parkview

Image and Character

Parkview is shaped by the greenways that bookend it, the prominent Summit Park frontage, and the access to Reed Hartman Via Carver Woods. It's form is predominantly 2-7 stories with a variety of street frontage types based on the building's use or uses. Parkview is the largest of the mixed-use neighborhoods by area and, therefore, will have many special places within it that are differentiated by their public space programming, the scale of development, and approach to architecture.



Winston-Salem, NC Condo



Seattle, WA



VENICE BEACH CONDO - Los Angeles, CA



Seattle, WA



LAKEWOOD BALMORAL - Chicago, IL



HERRNSTRASSE BUILDING - Munich, Germany



NORTON COMMONS - Prospect, KY

Golf View [±30 Acres]

Golf View is a premier residential neighborhood inspired by neo-traditional neighborhood design. This includes alley-loaded garages to prioritize high-quality neighborhood streetscapes and connection to the ample green spaces of Summit Park. It is envisioned to include:

- 90 to 110 single-family detached homes
- Rear garages, some with living spaces
- Front porches and outdoor living areas
- Managed garden spaces
- Connections to Summit Park



Golf View

Image and Character

Golf View is influenced by the timeless architectural traditions in Midwestern cities and towns. The streetscape is greeted with porches rather than garages, well-managed gardens instead of vast lawns, intimate shared spaces, and signature addresses along Plainfield Road, the greenways, and Summit Park. The houses will be 2 stories with a mix of brick and siding facades.



WESTHAVEN - Franklin, Tennessee



VICKERY VILLAGE - Cumming, Georgia



NORTON COMMONS - Louisville, Kentucky



Carver Woods [±18 Acres]

Carver Woods presents the opportunity to deliver a modern office environment that transitions from the existing legacy office space along the Carver Woods/Reed Hartman corridor into the Summit Park. It is intended to attract:

- Existing office park: ±90,000 square feet of existing single-story office (Carver Woods Executive Center) to be utilized in the near and intermediate term as "incubator" office product
- Class A office: 100,000 – 200,000 square feet of Class A office between three and five stories in height
- Retail/restaurant: 5,000 – 20,000 square feet of retail and restaurant space



LINDEN POINT - Norwood



MEDPACE - Cincinnati

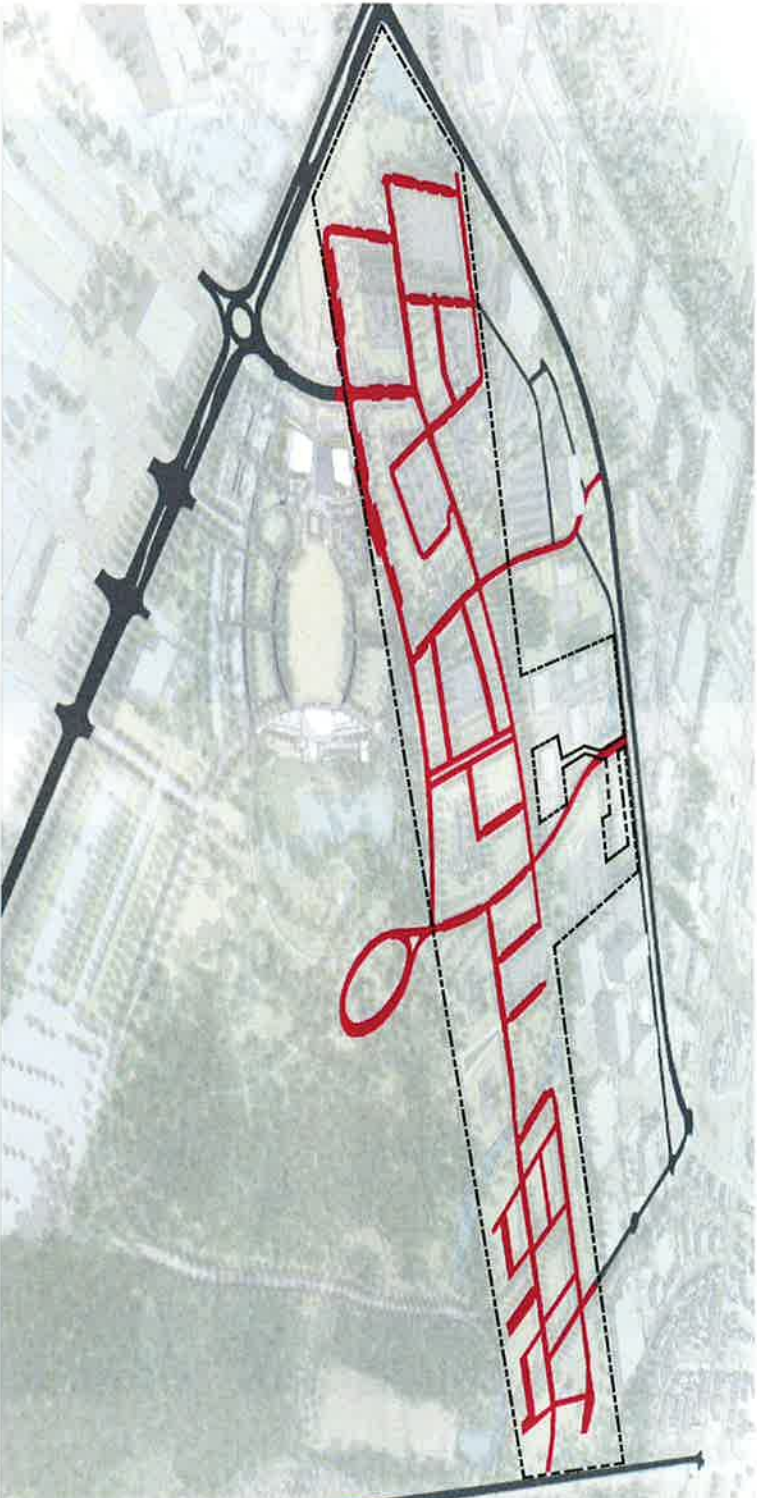


COLUMBIA SQUARE - Cincinnati, OH



THE FOUNDRY - Indianapolis, IN

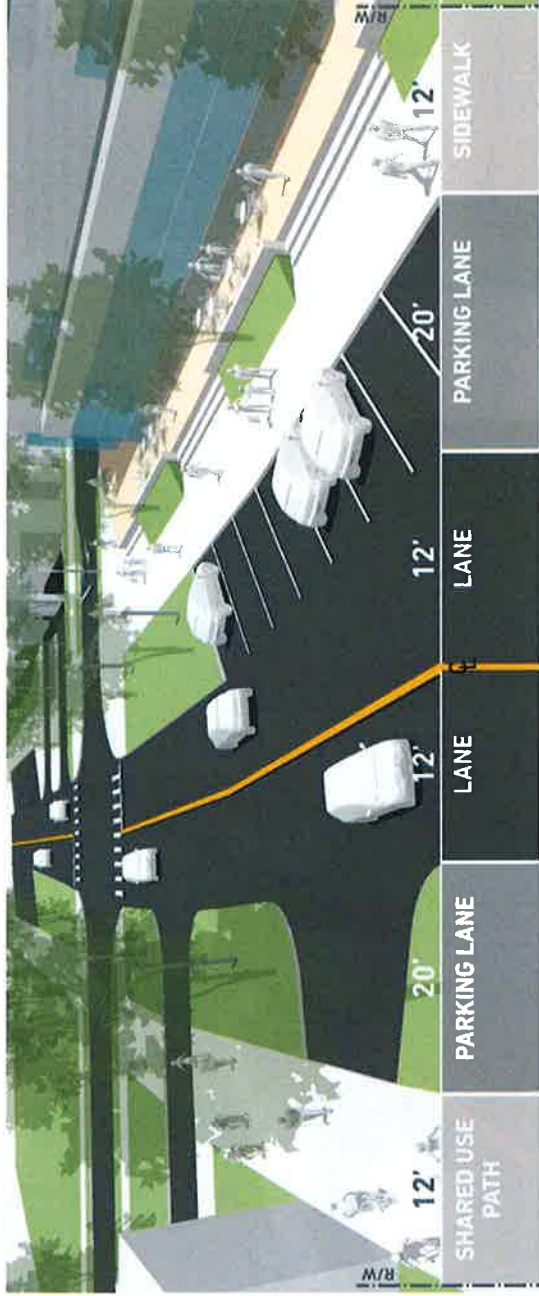
Streets



Whereas Summit Park is the primary public space for the City, the streets are the primary form of public space in Summit Park. As such, they will be designed, articulated, and comfortable for pedestrians and different types of users rather than as simply the conveyor of automobiles. They will feature narrow travel lanes, meandering bike trail, tree planting at regular intervals, lighting, and comfortable sidewalks. Intersections will be well marked and give primacy to the pedestrian and bike. Certain streets may sometimes be closed off for festivals and other community events.

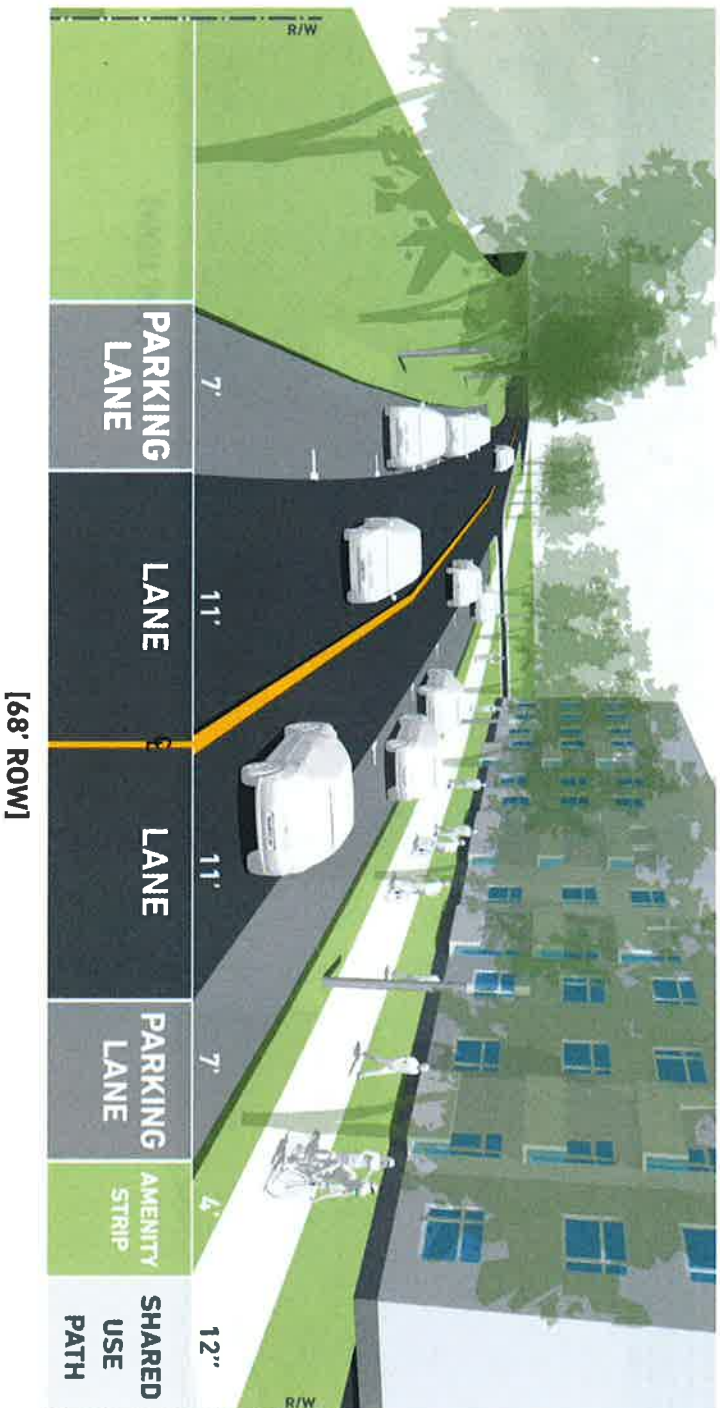
Summit Parkway

A continuous park edge will be created in the form of a public park drive. In the Gateway District, the park drive will feature diagonal parking, dining terraces, and mid-block pedestrian crossings. The design of the street will encourage travel speeds below 25 miles per hour.



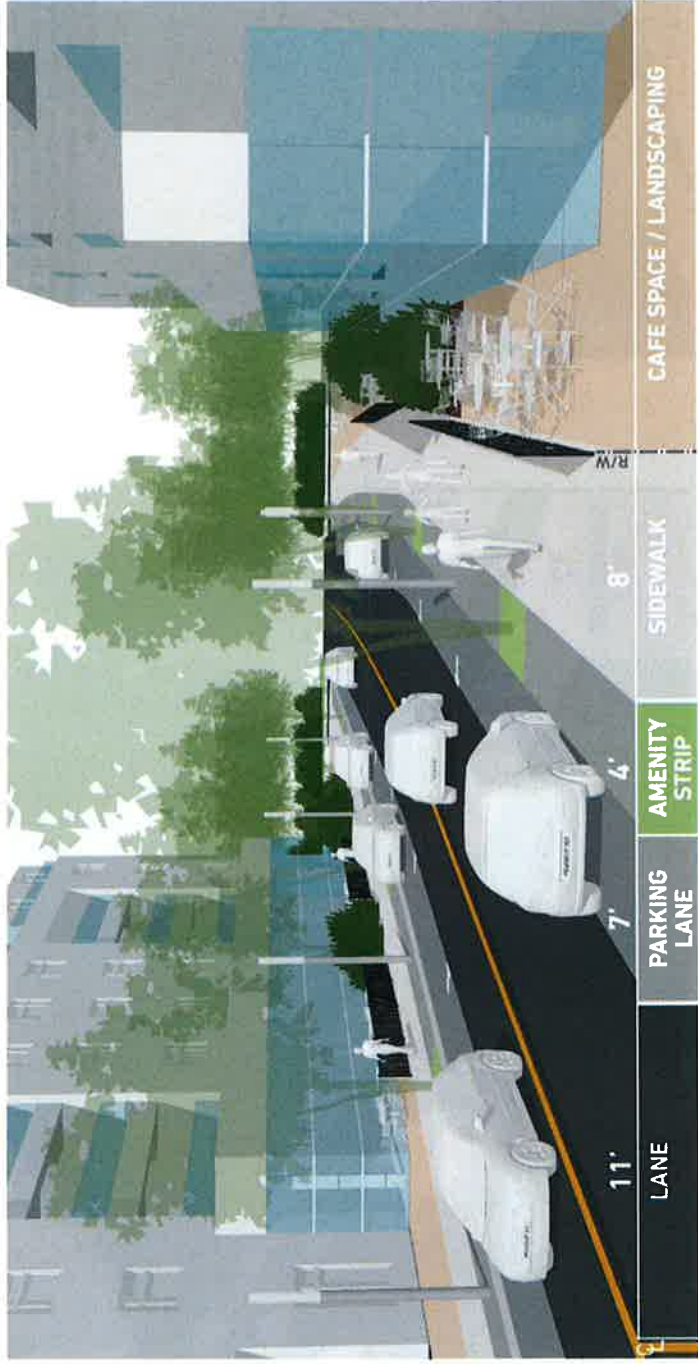
Summit Parkway

As the park drive moves south and west out of the mixed-use Gateway District, its character is modified and simplified to serve as an elegant front door of the Parkview neighborhood out to Summit Park. It is designed to discourage speeds over 30 miles per hour.



Carver Woods Drive

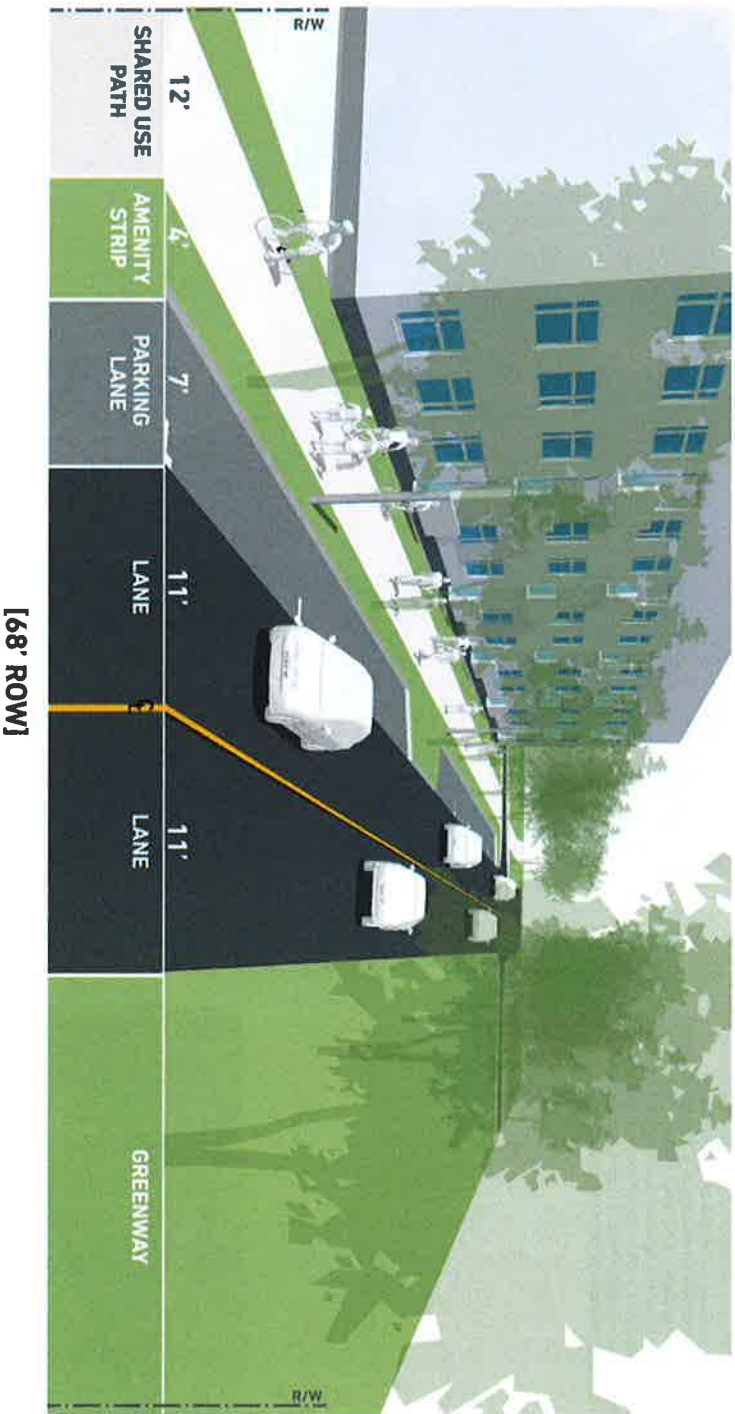
Summit Park is accessed off of Reed Hartman Highway via a new parkway connection that winds toward Summit Park before terminating at it. This street will be designed to facilitate a wide range of users, encourage bicycle transportation between Summit Park and the heart of Blue Ash, and be comfortably walkable within the Parkview Neighborhood. It is designed to encourage travel speeds below 30 miles per hour.



[60' ROW]

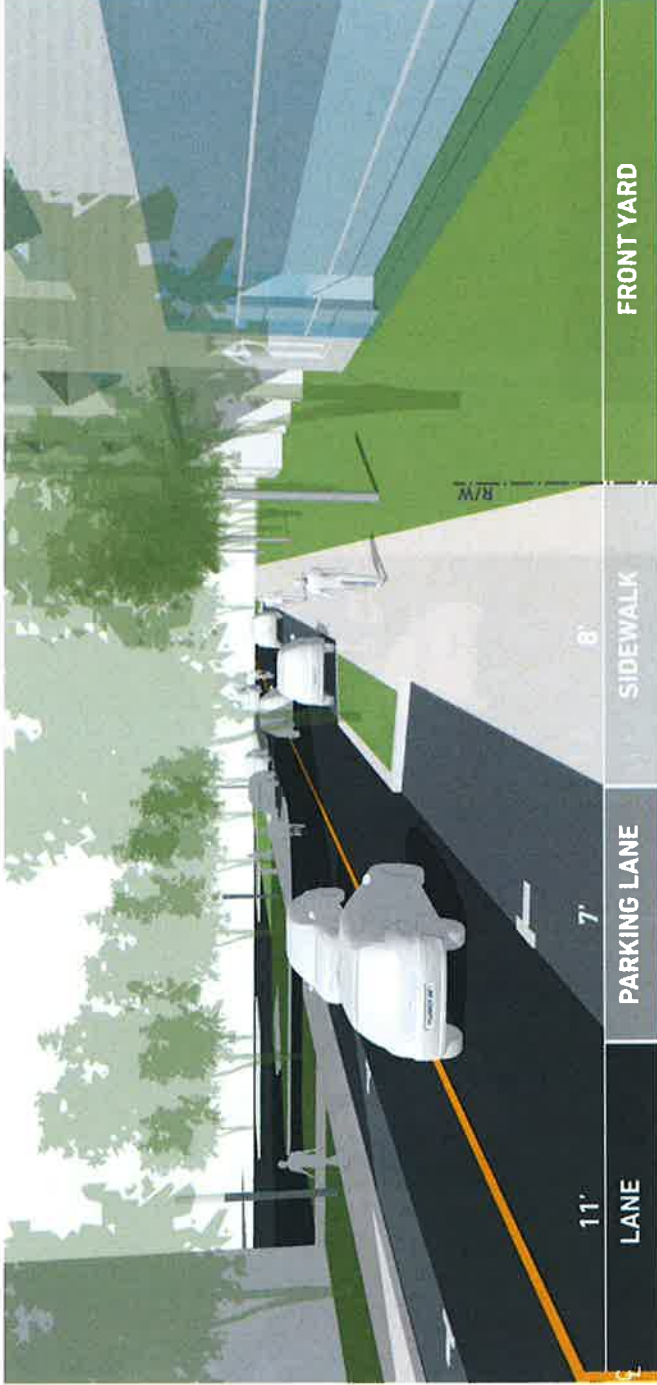
Gateway Drive

Internal Street D links the Gateway District to Parkview along the southern edge of the site. It will have sidewalks and on-street parking. The street will encourage speeds below 35 miles per hour. Green infrastructure will be explored south of the street.



Gateway Drive

The core of the office campus will be connected to the rest of the Gateway District via a street with sidewalks, trees in grades, and on-street parking. While fairly automobile-oriented in the near term, this street is designed to have more development face on to it as parking uses transition to building frontages.



[52' ROW]

Parkview Drive and Golf View Lane

Beautiful streets are essential to the success of the traditional neighborhood development of Golf View. The tree-lined streets of Golf View will have planting strips, sidewalks, lighting, and on-street parking. Intersections will have "bump outs" to calm traffic and provide easier pedestrian crossing. Residential streets will be designed to limit travel speeds to 15 miles per hour.



Parks & Public Spaces



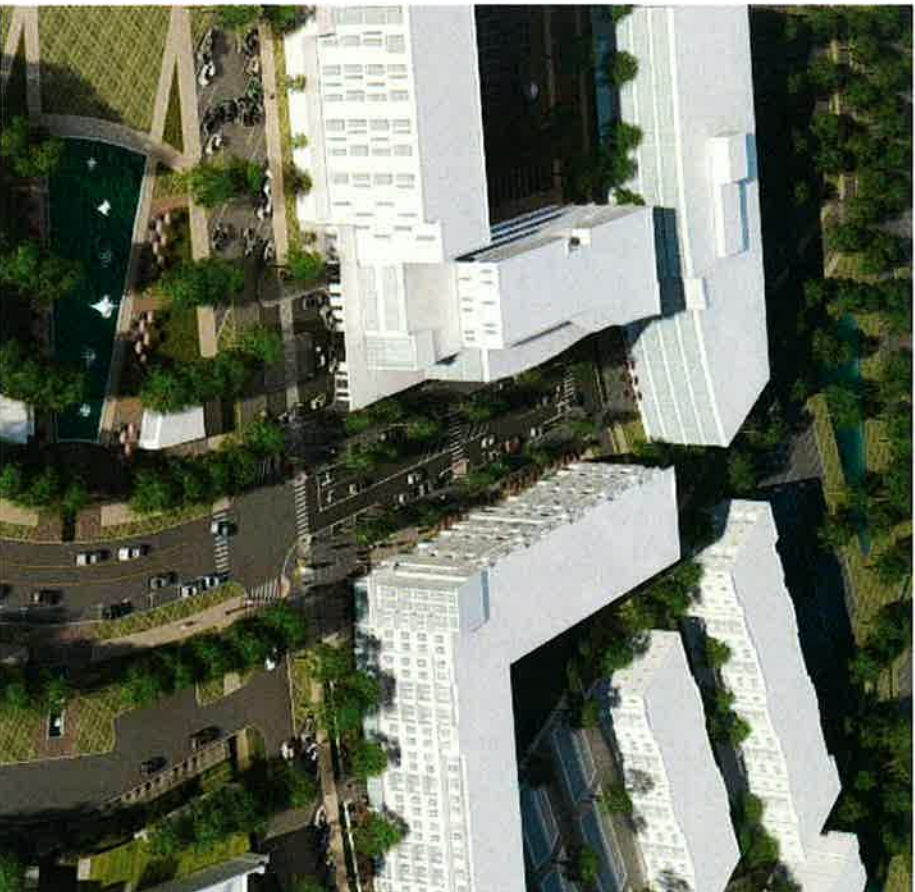
The organization and hierarchy of neighborhood parks and public spaces are closely linked to Summit Park Summit Park. Each neighborhood's major public spaces draw their identity from and connect directly to the various precincts of the park. They are designed not to compete in scale or use with the park but, rather, complement the regional amenity with smaller spaces that can provide different experiences. Greenways that daylight natural drainage on the site help distinguish different neighborhoods at Summit View. They will offer trails, native plantings, storm water storage, and a variety of natural features that enhance the sustainability of the overall development and facilitate passive use and enjoyment. Gardens, paseos, and courts provide memorable and well-managed landscapes within blocks. Plazas, widened sidewalks, and shared-surface streets provide a more structured, urban experience that may be programmed with a wide variety of uses. Trails and pathways connect all the neighborhoods of Summit View to each other and to Summit Park.



Parks & Public Spaces

Summit Place

Summit Place will be the social and cultural core of Gateway Village. Cafés and dining venues lining the street will offer intimate outdoor seating areas along a richly appointed streetscape. Specialty pavements will be used to create a pedestrian-centric experience extending from Gateway Drive north and east to the Park's Main Street.



Parks & Public Spaces

Sycamore Creek Greenway (East)

The Sycamore Creek Greenway offers expansive views over the northwest branch of Sycamore Creek and integrates the park's natural riparian landscape with walking trails and storm water quality basins that collect and filter run-off from surrounding streets. Creekside Drive will frame the greenway and offer picturesque views toward Gateway Village and the park's observation tower.



MCFERSON COMMONS - Columbus, Ohio

Parks & Public Spaces

Parkview Commons

Parkview Commons will serve as the Parkview Neighborhood's community green and the focal point of neighborhood gatherings and events. The Carver Woods Drive opens up to a sweeping formal lawn. The street and lawn will be framed by 4 to 5 story buildings with dramatic vistas over the Park's "2nd Tier" entrance loop to the pond and meadows beyond.



ARENA DISTRICT - Columbus, Ohio

Summit Park Land Development



Parks & Public Spaces

West Greenway and Golf View Commons

The West Greenway weaves the park's riparian landscape through the Parkview and Golfview neighborhoods and links the park's pathway system with neighborhood streets to the east and west.

Golfview Commons serves as both the Golfview neighborhood's central gathering space and the trailhead to the Park's "Frontier". Frontier Court provides framed views along a landscaped boulevard and is designed to hold neighborhood socials, block parties and garden club events.



Green Infrastructure

Stormwater Detention Facilities

Bio-infiltration detention facilities will provide temporary storage and treatment of stormwater run-off. They may be either "Dry" or "Wet" Basins planted with native grasses and forbs.

Dry extended detention basins maximize an area's available flood storage capacity while providing extended detention to promote the settling and entrapment of suspended pollutants. Run-off from larger storm events is stored across a grassed or planted basin which is sloped to drain down over a period of 24 to 48 hours. This extended detention area can be designed with an engineered soil to provide enhanced filtration and infiltration of run-off or can be maintained in a native soil with either permeable or non-permeable properties.

Wet detention basins are stormwater quality basins that hold a permanent pool of water throughout the year. These ponds treat incoming stormwater runoff by allowing pollutant particles to settle and promote nutrient uptake through biological activity in and around the permanent pool.

Detailed design of stormwater management facilities will be provided as part of subsequent final development plans.



BIO-INFLTRATION & DETENTION AREAS



DRY BASIN



WET BASIN



VEGETATED SWALE

NO.	DESCRIPTION	DATE



CARDINAL
ENGINEERING
AND SURVEYING
 302 W. 3RD STREET, SUITE 800
 CINCINNATI, OHIO 45202
 PHONE: 513.963.2400
 FAX: 513.963.2404

WEBSITE: <http://www.cardinal-engineering.com>

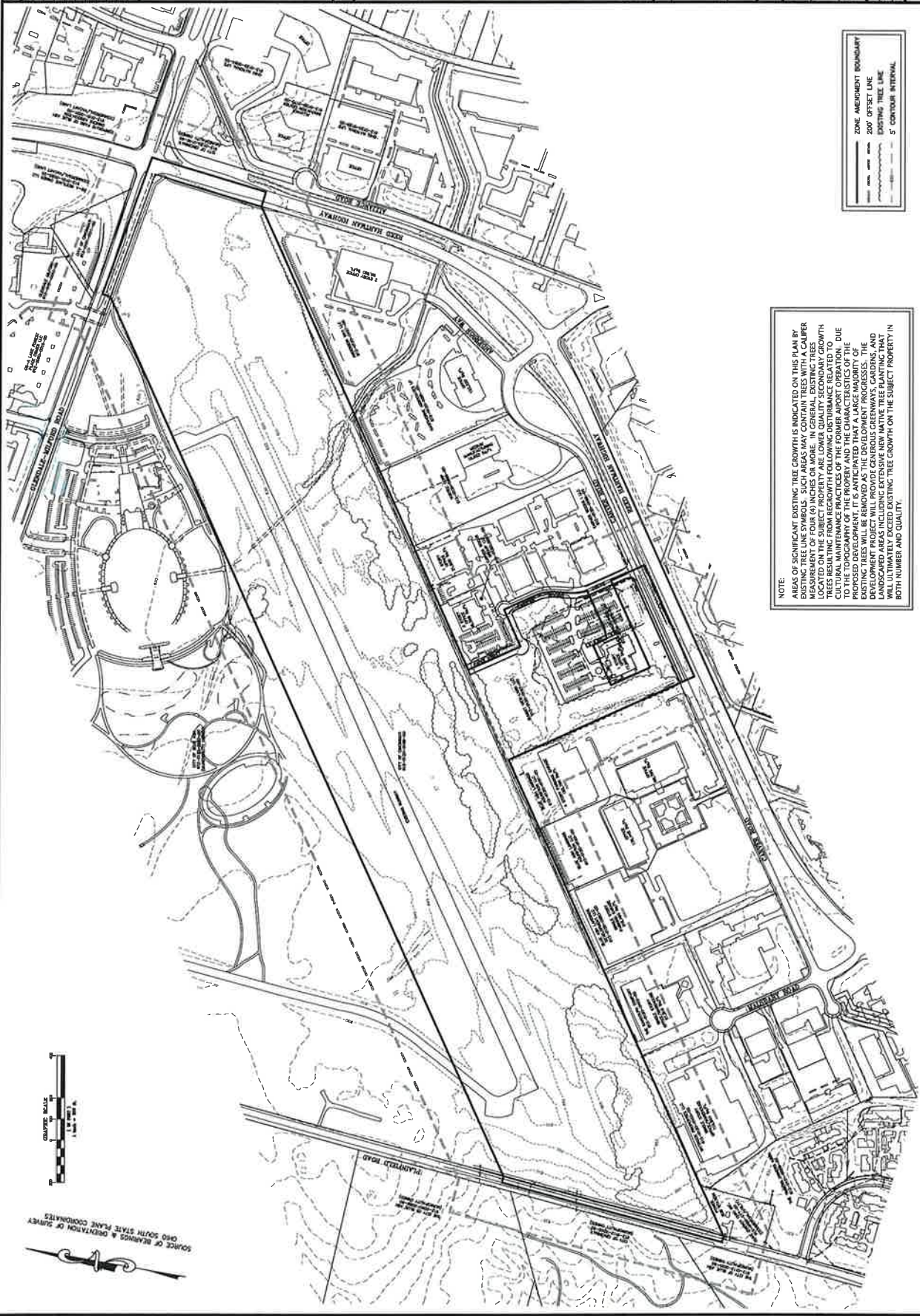
PROJECT: SUMMIT PARK LAND DEVELOPMENT
 CLIENT: AL NEYER LLC
 302 W. 3RD STREET, SUITE 800
 CINCINNATI, OHIO 45202
 VANDERCAR
 5027 MADISON ROAD, SUITE 200
 CINCINNATI, OHIO 45227



VICINITY MAP
 N.T.S.

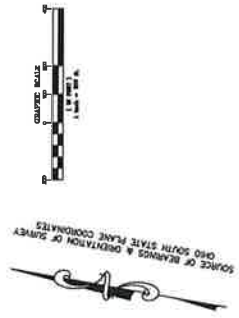
PROJECT NO.	16-001-03
SCALE	1" = 200'
DATE	07-12-16
EXISTING CONDITION PLAN	

SHEET
C-2.0



NOTE:
 AREAS OF SIGNIFICANT EXISTING TREE GROWTH IS INDICATED ON THIS PLAN BY EXISTING TREE LINE SYMBOLS. SUCH AREAS MAY CONTAIN TREES WITH A CALIPER DBH OF 10" OR GREATER. TREES WITH A CALIPER DBH OF 10" OR GREATER LOCATED ON THE SUBJECT PROPERTY ARE LOWER QUALITY SECONDARY GROWTH TREES RESULTING FROM RECOVERY FOLLOWING DISTURBANCE RELATED TO CULTURAL MAINTENANCE PRACTICES OF THE FORMER AIRPORT OPERATION. DUE TO THE PROPOSED DEVELOPMENT, IT IS ANTICIPATED THAT A LARGE MAJORITY OF EXISTING TREES WILL BE REMOVED AS THE DEVELOPMENT PROGRESSES. THE DEVELOPMENT PROJECT WILL PROVIDE CONTOUR GREENWAYS, GARDENS, AND PLANTINGS TO REPLACE EXISTING TREES. THE DEVELOPMENT PROJECT WILL ULTIMATELY EXCEED EXISTING TREE GROWTH ON THE SUBJECT PROPERTY IN BOTH NUMBER AND QUALITY.

---	ZONE AMENDMENT BOUNDARY
---	200' OFFSET LINE
---	EXISTING TREE LINE
---	5' CONTOUR INTERNAL



DATE	1/11/2017
DESCRIPTION	REVISED
BY	JL
CHECKED BY	JL
APPROVED BY	
DATE	
DESCRIPTION	
BY	
CHECKED BY	
APPROVED BY	
DATE	

CARDINAL
ENGINEERING
AND SURVEYING
INCORPORATED
 302 W. 3RD STREET, SUITE 800
 CINCINNATI, OHIO 45202
 PHONE: 954.941.4600
 FAX: 954.941.4616
 WWW: WWW.CARDINALENGINEERING.COM

PROJECT: SUMMIT PARK LAND DEVELOPMENT
CLIENT: AL NEYER LLC
 302 W. 3RD STREET, SUITE 800
 CINCINNATI, OHIO 45202
 VANDERKAR
 5027 MADISON ROAD, SUITE 200
 CINCINNATI, OHIO 45227

VICINITY MAP
 N.T.S.

PROJECT NO.: 16-003-03
SCALE: 1" = 200'
DATE: 07-12-16
OVERALL PLAN
SHEET C-3.0

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NOTES:
 1. THE INTENT OF THIS PLAN TO INDICATE LOCATION, USE AND ARRANGEMENT OF THESE AREAS ASSOCIATED WITH THE PROPOSED DEVELOPMENT TO CONSTRUCTION OF ANY AREA OF THE PROJECT, THE DEVELOPER SHALL SUBMIT FOR THE CITY'S REVIEW AND APPROVAL DETAILED PLANNING PLANS AND DETAILS OF SUCH AREA.
 2. PRIOR TO CONSTRUCTION OF ANY PORTION OF THE PROJECT, THE DEVELOPER SHALL SUBMIT FOR THE CITY'S REVIEW AND APPROVAL A COMPREHENSIVE SIGNAGE PLAN, INCLUDING SIGNAGE LOCATION AND DESIGN AS WELL AS DETAILED SIGNAGE PLANS FOR ANY PROPOSED CONSTRUCTION.
 3. THE OVERALL PROJECT WILL BE CONSTRUCTED IN PHASES. IT IS THE INTENT OF THIS PLAN TO ADDRESS THE PHASE 1 DEVELOPMENT WHICH INCLUDES CONSTRUCTION ASSOCIATED WITH GATEWAY VILLAGE, PHASE 1, CONSISTING OF MIXED USE AND MULTI-FAMILY RESIDENTIAL BUILDINGS, 1, 2 AND 3, OFFICE AND COMMERCIAL BUILDINGS, AND A COMMUNITY CENTER. PHASE 1 SINGLE-FAMILY RESIDENTIAL COMPONENT. IT IS THE DEVELOPER'S INTENT TO BEGIN CONSTRUCTION OF PHASE 1 IN THE SPRING OF 2017. THE TIMING OF ADDITIONAL PHASES OF THE DEVELOPMENT WILL BE MARKET DRIVEN WITH FULL BUILD OUT ANTICIPATED TO BE GREATER THAN TEN YEARS FROM PROJECT START.



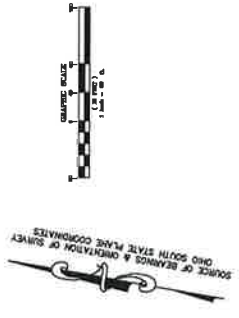
GOLFVIEW DISTRICT - PHASE 1

TOTAL AREA = 33.1 AC +/-

LOT SCHEDULE:

46 Foot Lot Type	14 Lots
50 Foot Lot Type	19 Lots
55 Foot Lot Type	39 Lots
60 Foot Lot Type	30 Lots
TOTAL =	102 Lots

NOTES:
 1. TYPICAL LOT DEPTH = 120 FT
 2. TYPICAL STREET RIGHT-OF-WAY = 60 FT
 3. TYPICAL ALLEY EASEMENT = 16 FT



REVISED	DATE

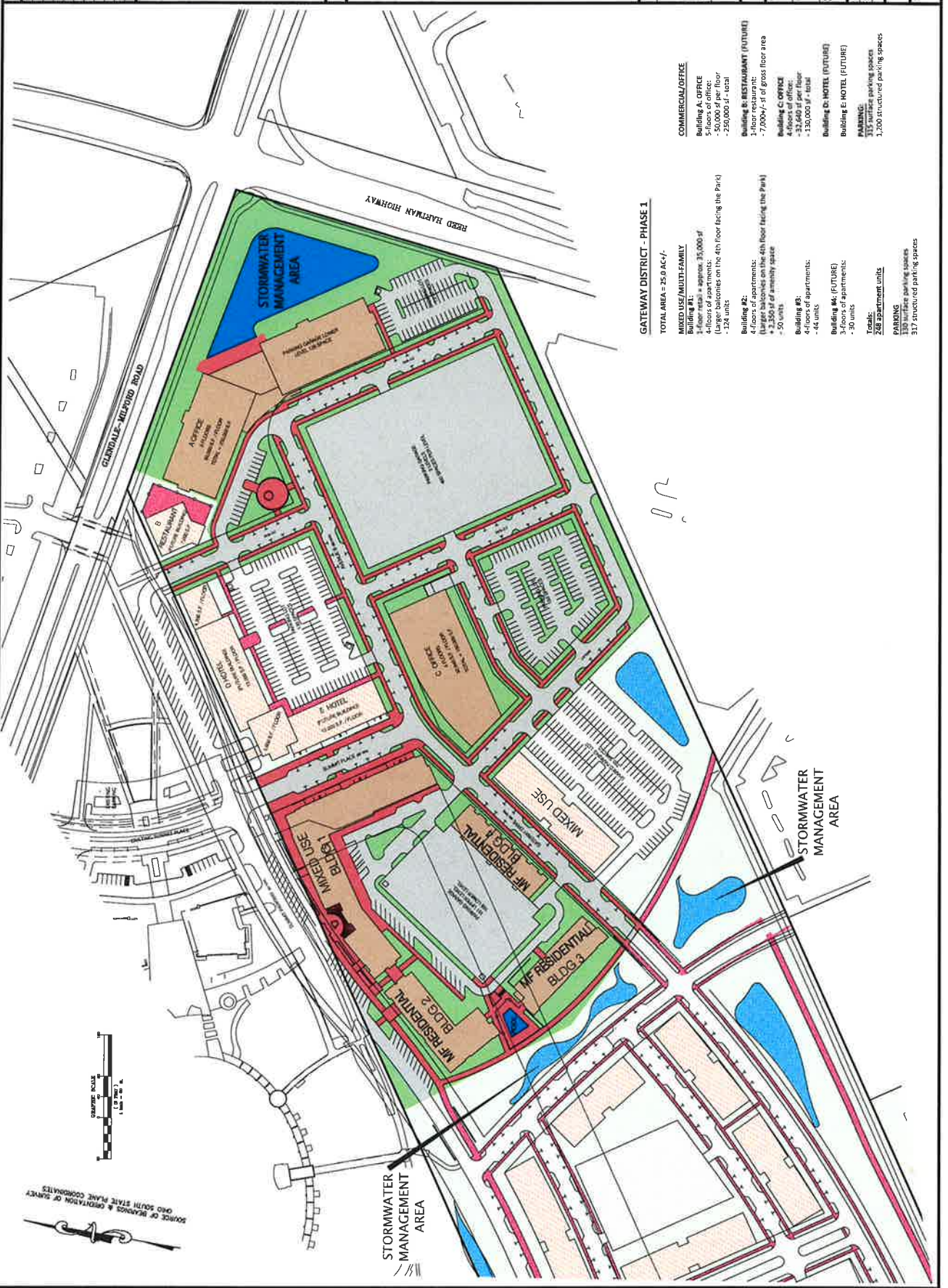

CARDINAL
 ENGINEERING
 4000 SUBURBAN
 CINCINNATI, OHIO 45241
 PHONE 513.951.4000
 FAX 513.951.4000

PROJECT: SUMMIT PARK LAND DEVELOPMENT
 ZONE CHANGE PLAN
 CLIENTS: AL NEYER LLC
 302 W. 3RD STREET, SUITE 800
 CINCINNATI, OHIO 45202
 VANDERCAR
 5027 MADISON ROAD, SUITE 200
 CINCINNATI, OHIO 45227



PROJECT NO.	16-003-03
SCALE	1" = 80'
DATE	07-12-16
PHASE	PHASE 1
GATEWAY DISTRICT	GATEWAY DISTRICT

SHEET **C-5.0**



GATEWAY DISTRICT - PHASE 1

TOTAL AREA = 25.0 AC +/-

MIXED USE / MULTI-FAMILY

Building #1:
 4 floors of offices: 35,000 sf
 4 floors of apartments:
 (Larger balconies on the 4th floor facing the park)
 - 124 units

Building #2:
 4 floors of apartments:
 (Larger balconies on the 4th floor facing the park)
 - 23,500 sf of amenity space
 - 50 units

Building #3:
 4 floors of apartments:
 - 44 units

Building #4: (FUTURE)
 3 floors of apartments:
 - 30 units

Totals:
 268 apartment units
 339 surface parking spaces
 317 structured parking spaces

COMMERCIAL/OFFICE

Building A: OFFICE
 5 floors of office:
 - 50,000 sf per floor
 - 250,000 sf - total

Building B: RESTAURANT (FUTURE)
 1-floor restaurant:
 - 7,000 sf - 5' of gross floor area

Building C: OFFICE
 4 floors of office:
 - 32,640 sf per floor
 - 130,560 sf - total

Building D: HOTEL (FUTURE)
 4 floors of hotel:

PARKING:
 315 surface parking spaces
 1,200 structured parking spaces

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REVISIONS	DATE	ITEM



CARDINAL
ENGINEERING
 302 W. 3RD STREET, SUITE 800
 CINCINNATI, OHIO 45202
 PHONE: (513) 581-0400
 FAX: (513) 581-0400
 WWW: WWW.CARDINAL-ENG.COM

PROJECT:
 SUMMIT PARK LAND DEVELOPMENT
 ZONE CHANGE PLAN

CLIENT:
 AL NEYER LLC
 302 W. 3RD STREET, SUITE 800
 CINCINNATI, OHIO 45202
 VANDERCAR
 5027 MADISON ROAD, SUITE 200
 CINCINNATI, OHIO 45227



VICINITY MAP
 IN U.S.

NO.	DATE	BY

PROJECT NO.: 16-003-03
SCALE: 1" = 200'
DATE: 07-12-16
OVERALL DRAINAGE

SHEET
C-6.0



SOURCE OF BEARS & ORIENTATION OF SURVEY
 AND SOUTH STATE PLANE COORDINATES



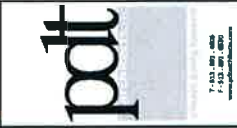
DATE PLOTTED: 07-12-16 10:45:11 AM
 PLOTTER: HP DesignJet 2450
 PLOT SCALE: 1" = 200'

DATE	07/27/2018
PREPARED BY	
DESIGNED BY	
CHECKED BY	
DATE	

COMMENTS

Towne Properties
1055 St. Paul Place
CINCINNATI, OH 45222

PROJECT ADDRESS
SUMMIT PARK LAND DEVELOPMENT
BLUE ASH, OH 45242



PRELIMINARY

DATE	07/27/2018
PREPARED BY	
DESIGNED BY	
CHECKED BY	
DATE	



APT BUILDING #1: LOOKING SOUTHWEST



APT BUILDING #1: LOOKING SOUTHEAST

DATE	07/27/2018
BY	ARCHITECT
REVISION	
NO.	
DESCRIPTION	
DATE	
BY	
REVISION	
NO.	
DESCRIPTION	

1055 Rt. 9504
 Cincinnati, OH 45202
Towne Properties

BLUE HAVEN, OH 45242
SUMMIT PARK LAND DEVELOPMENT

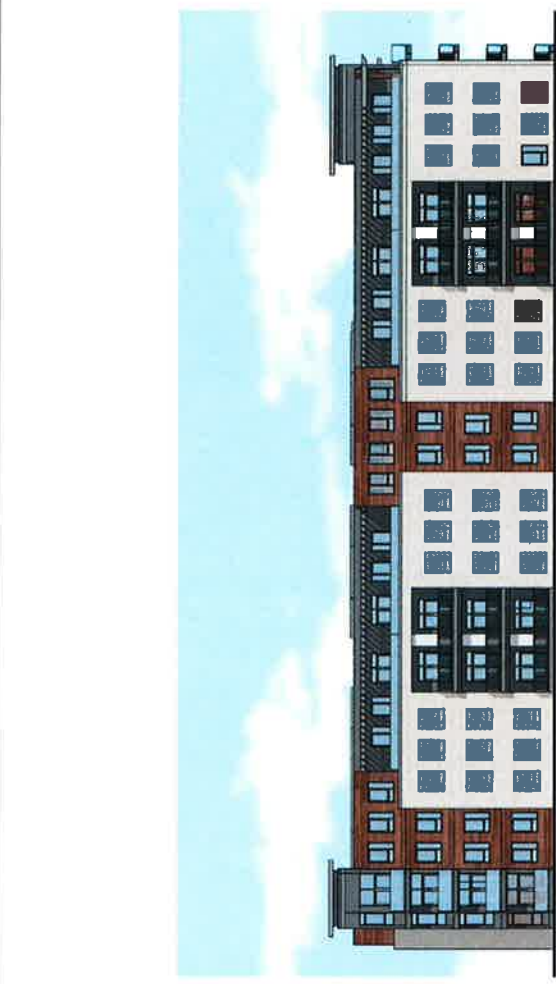


PRELIMINARY

DESIGNED BY	PDT
DRAWN BY	DTL
CHECKED BY	PTJ
DATE	7/27/18
PROJECT NO.	
COMMISSION NO.	PL18



APT BUILDING #2: LOOKING NORTHEAST



APT BUILDING #2: LOOKING SOUTHEAST



APT BUILDING #3: LOOKING NORTHEAST



SUMMIT PARK
OFFICE A VIEWS





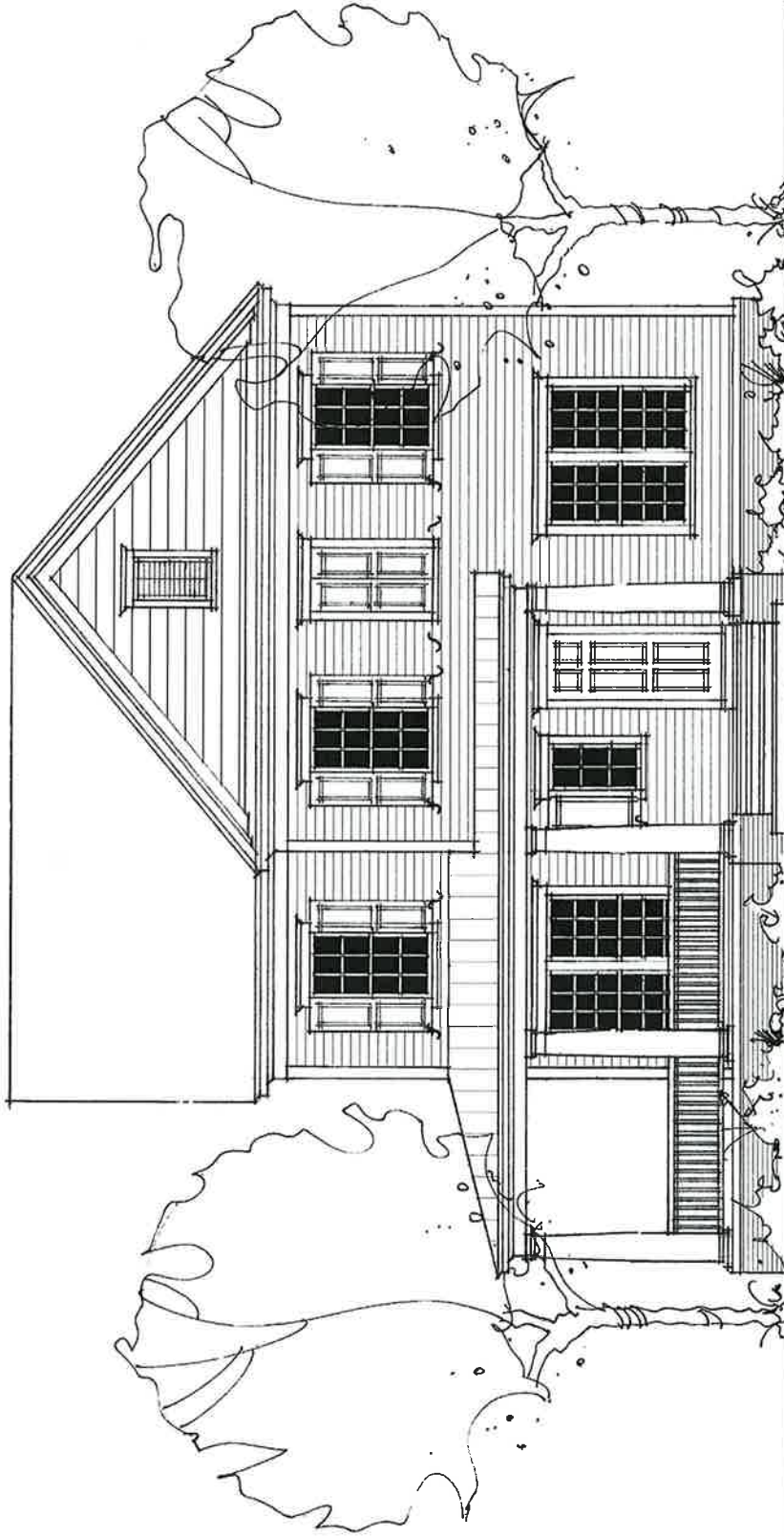
SUMMIT PARK
OFFICE BUILDING C



Golf View

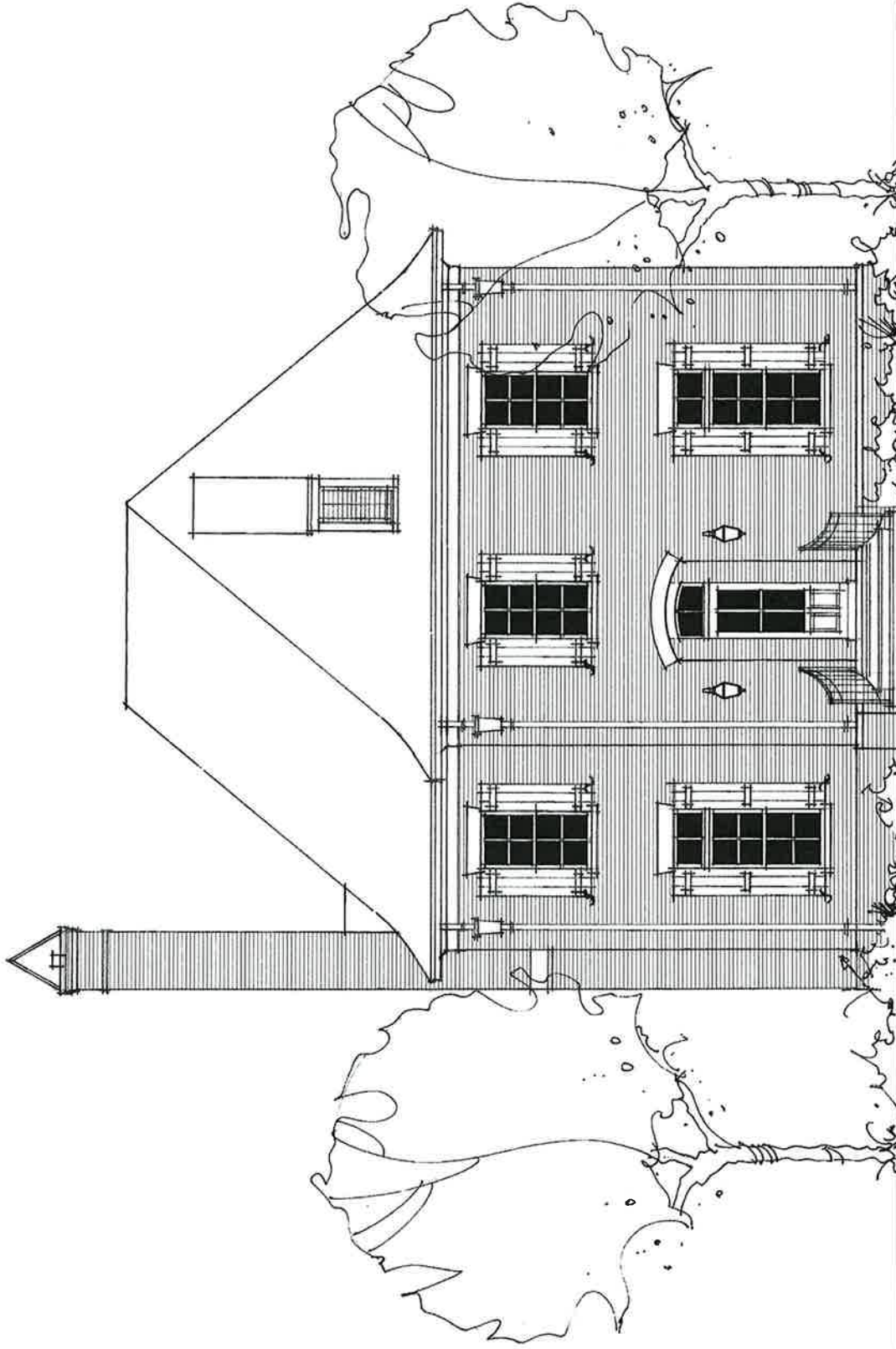


SINGLE-FAMILY RESIDENTIAL NEIGHBORHOOD: STREETSCAPE



ELEVATION A, "CLASSICAL"
RESOLUTE PLAN

BLUE ASH, OHIO
M/I HOMES, INC.
JUNE 21, 2016



ELEVATION B, "FRENCH"

RESOLUTE PLAN

BLUE ASH, OHIO

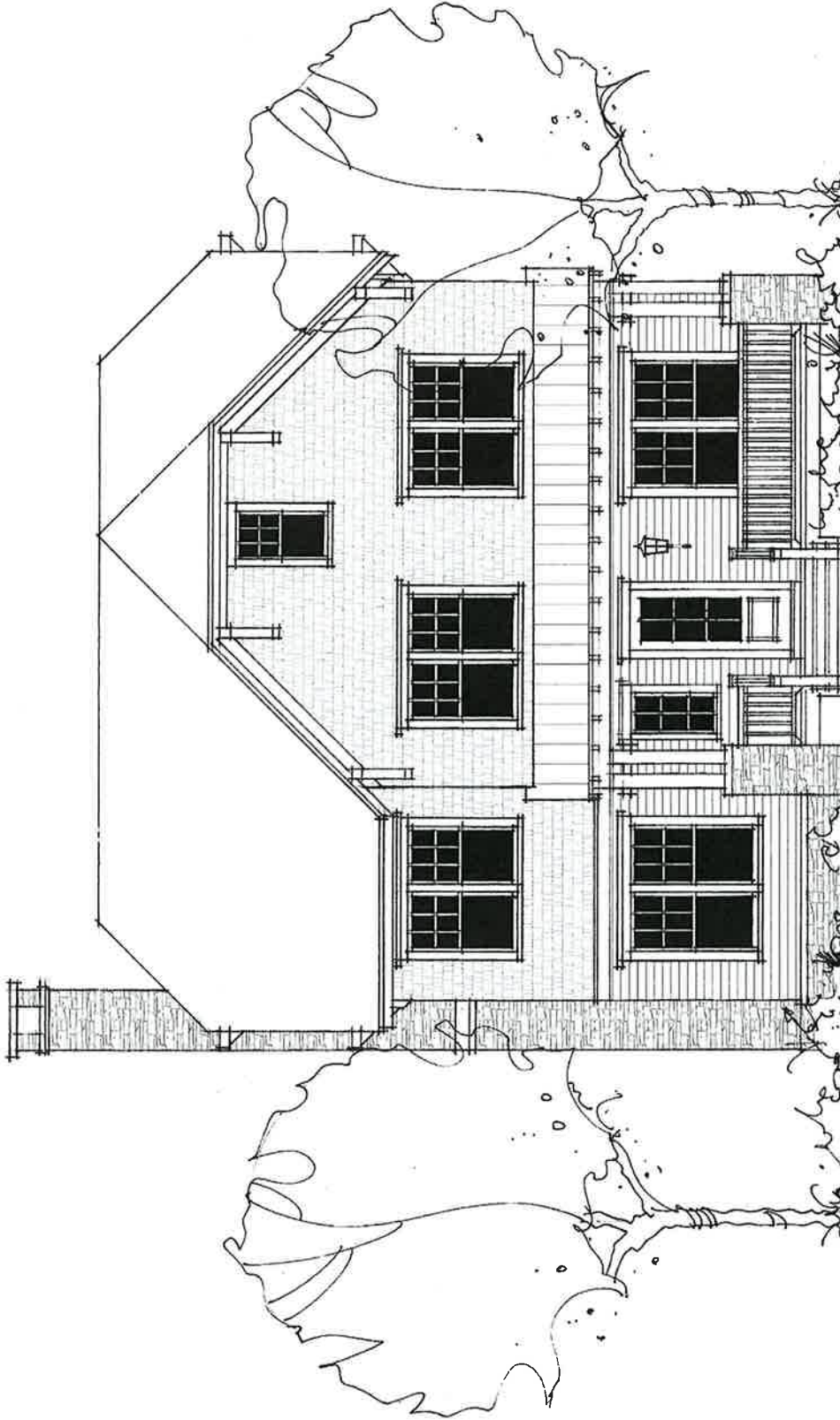
M/I HOMES, INC.

JUNE 21, 2016



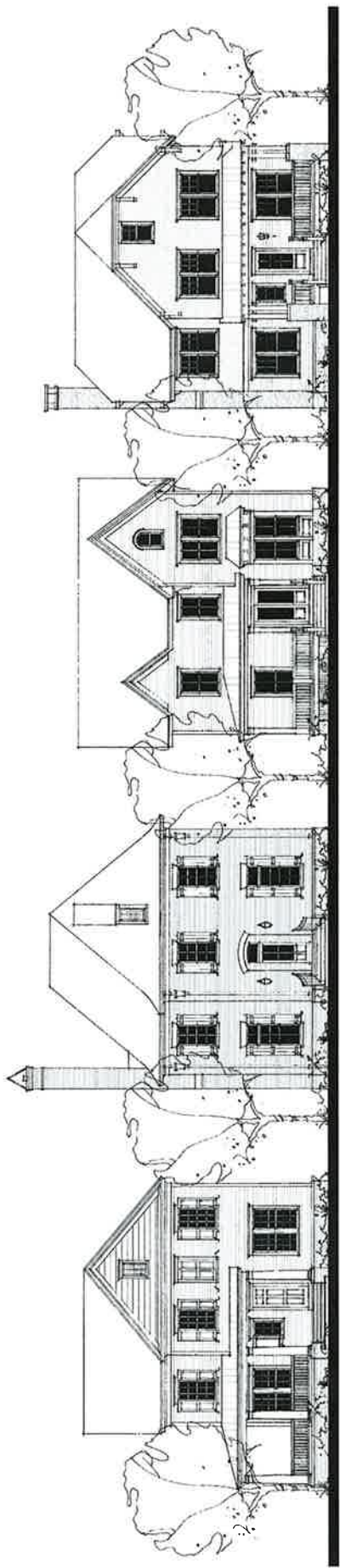
ELEVATION C, "VICTORIAN"
RESOLUTE PLAN

BLUE ASH, OHIO
M/I HOMES, INC.
JUNE 21, 2016

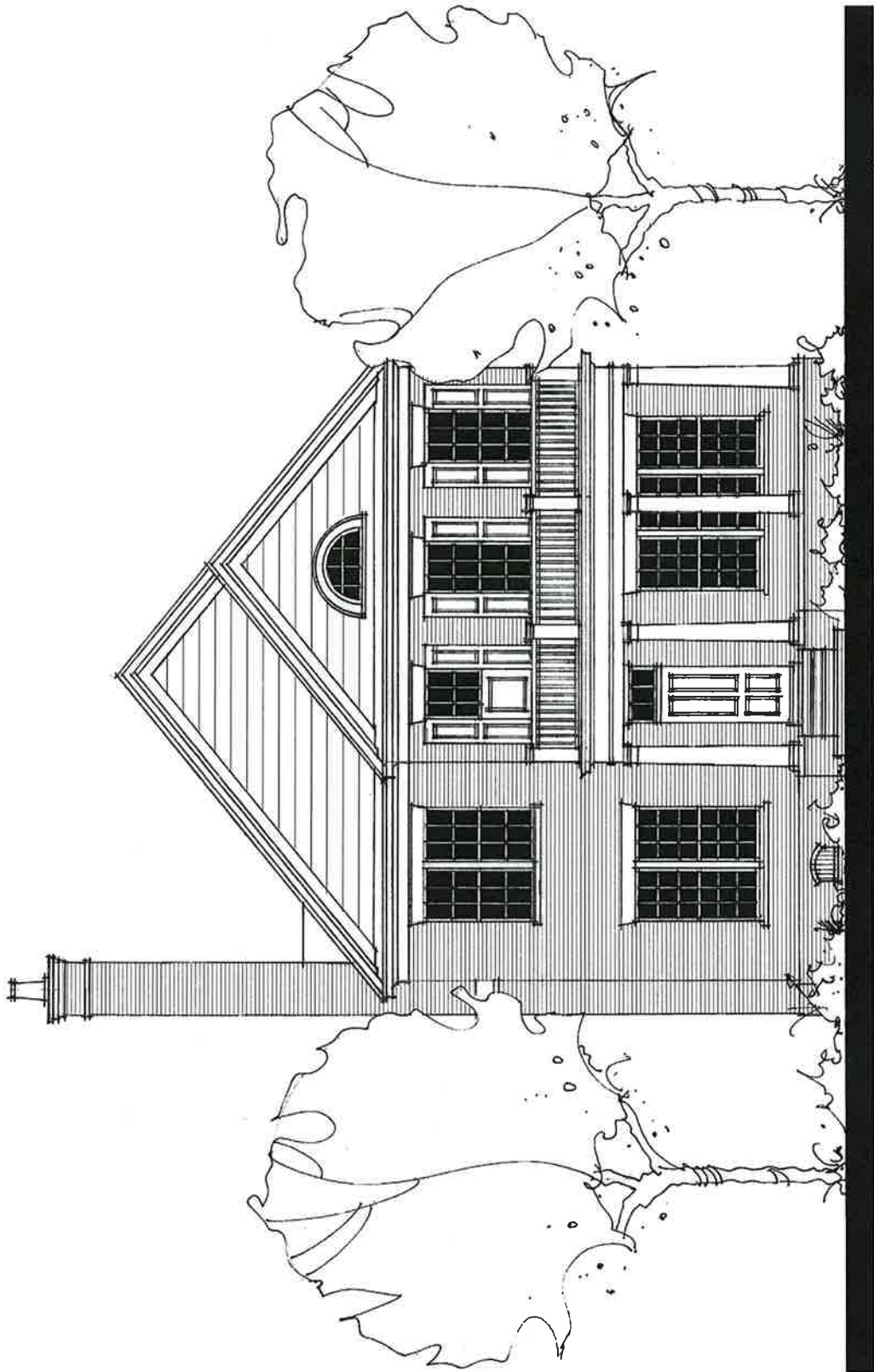


ELEVATION D, "CRAFTSMAN"
RESOLUTE PLAN

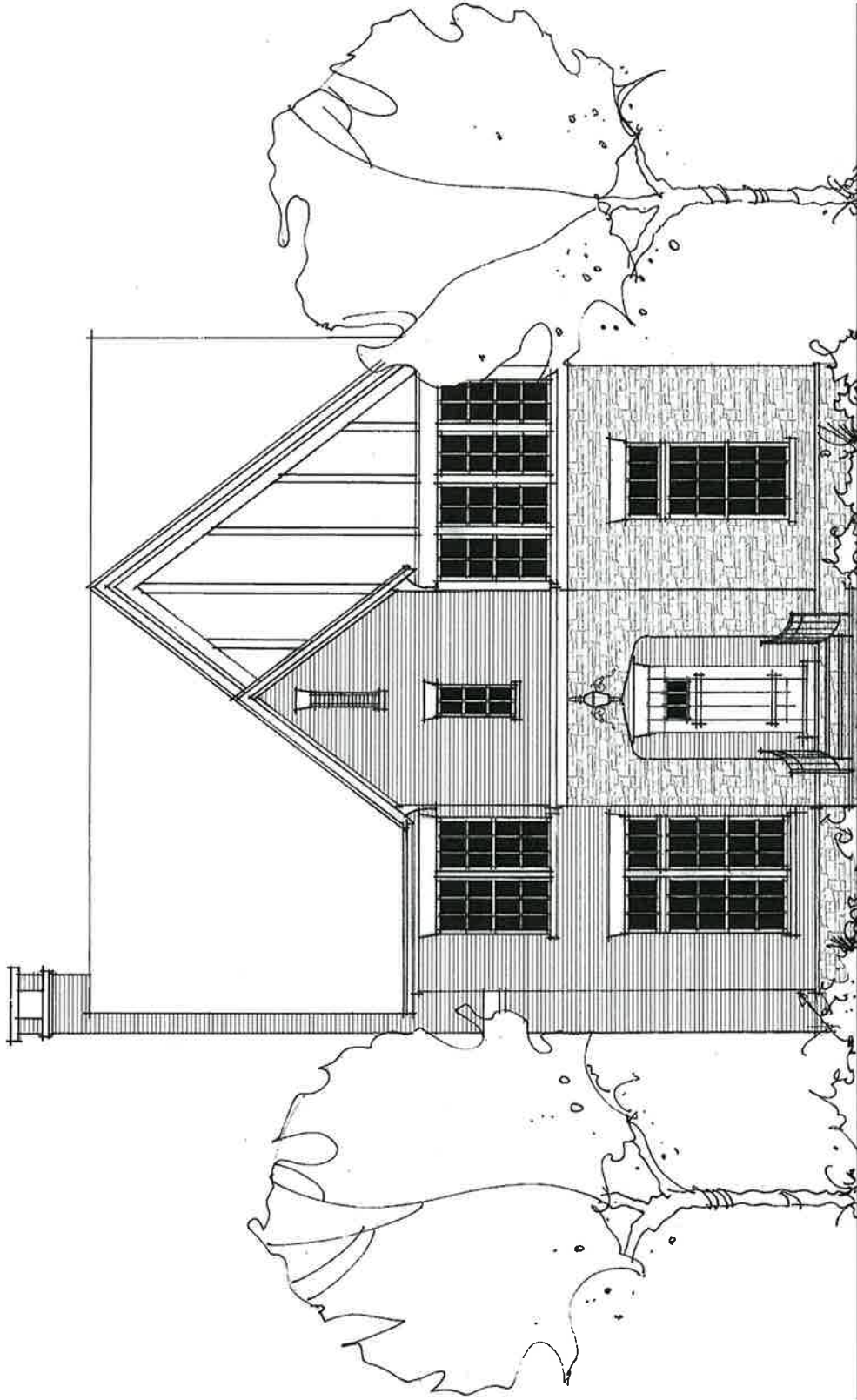
BLUE ASH, OHIO
M/I HOMES, INC.
JUNE 21, 2016



STREETSCAPE, RESOLUTE PLAN
BLUE ASH AIRPORT
BLUE ASH, OHIO
M/I HOMES, INC
JUNE 21, 2016



ELEVATION A, "CLASSICAL"
ALTIRA PLAN
BLUE ASH, OHIO
M/HOMES, INC.
JUNE 16, 2016



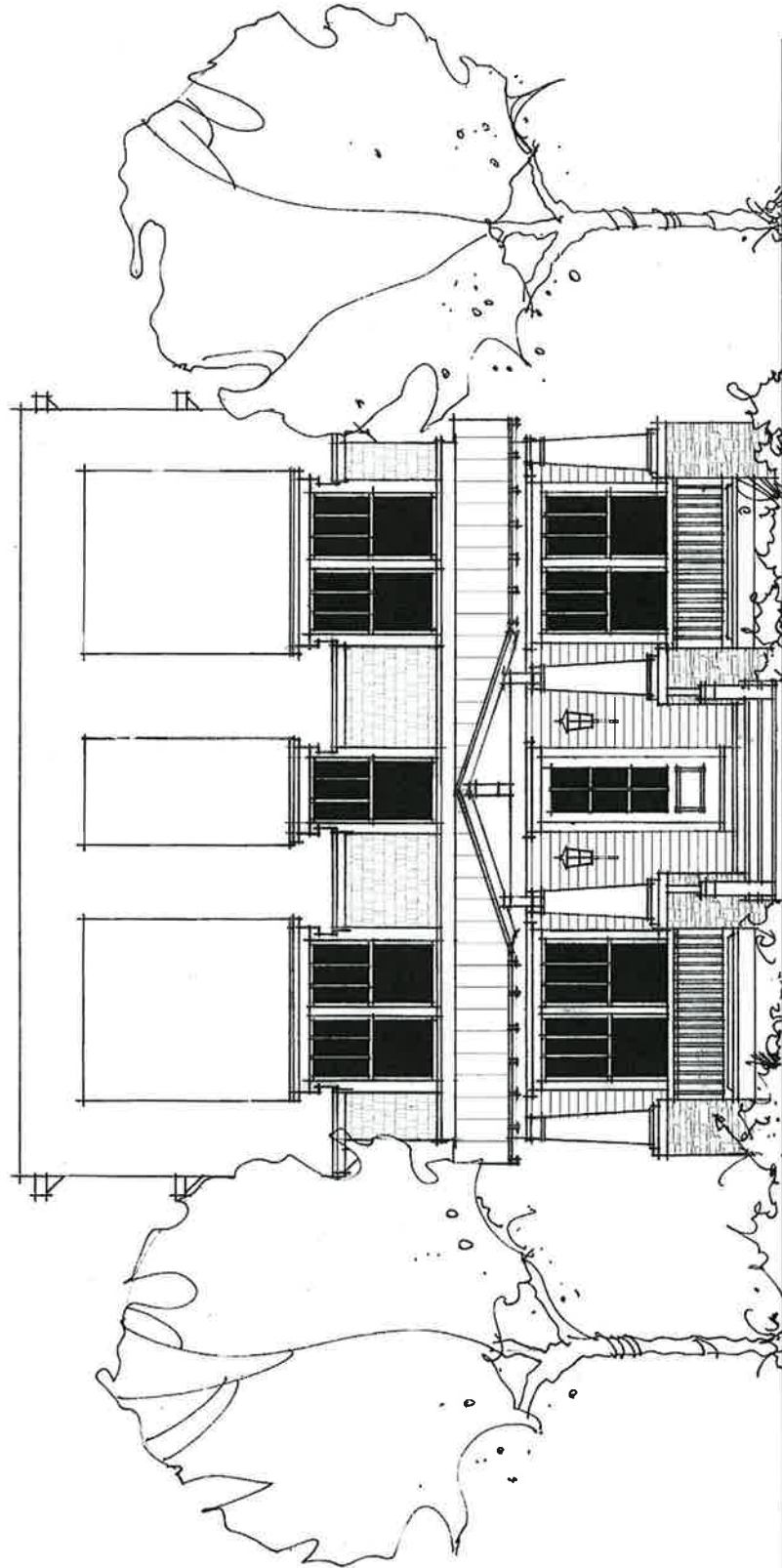
ELEVATION B, "EUROPEAN"

ALTIRA PLAN

BLUE ASH, OHIO

M/I HOMES, INC.

JUNE 16, 2016



ELEVATION C, "CRAFTSMAN"

ALTIRA PLAN

BLUE ASH, OHIO

M/I HOMES, INC.

JUNE 16, 2016



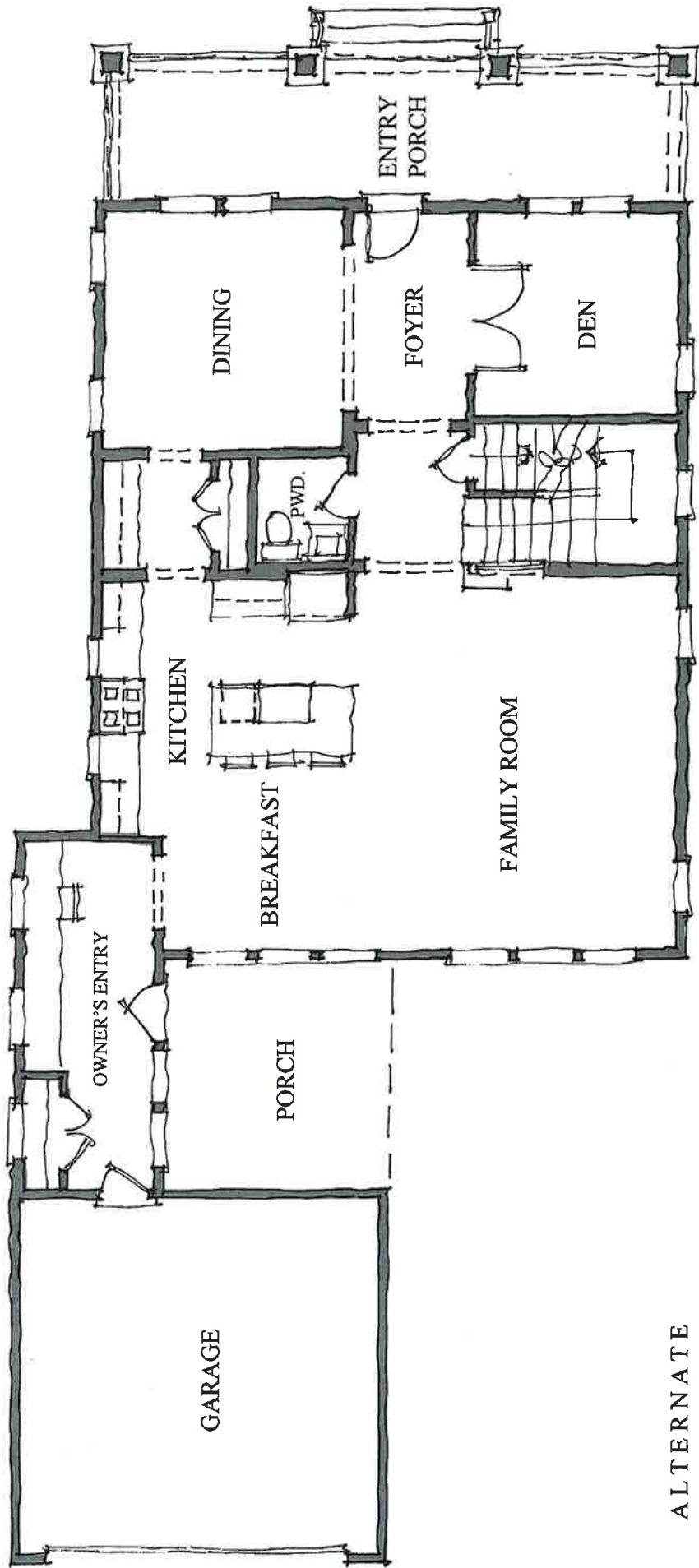
ELEVATION D, "FARMHOUSE"

ALTIRA PLAN

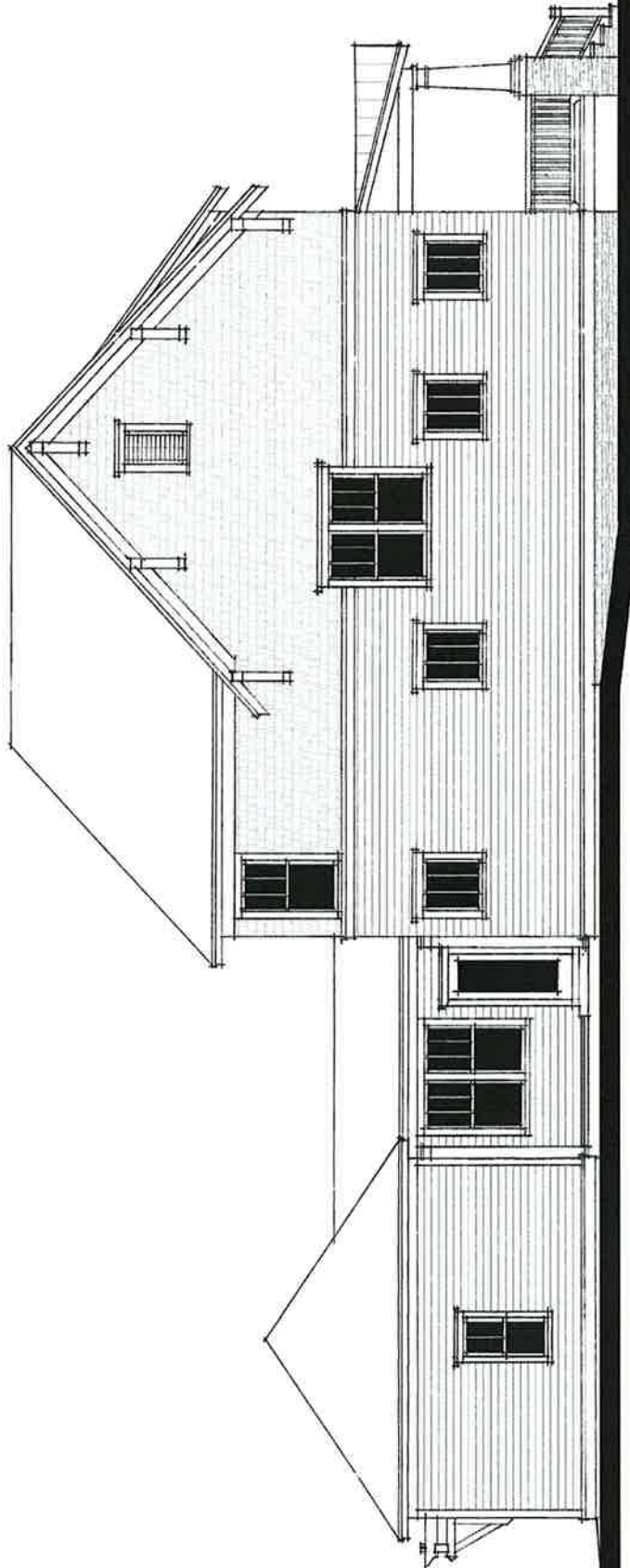
BLUE ASH, OHIO

M/I HOMES, INC.

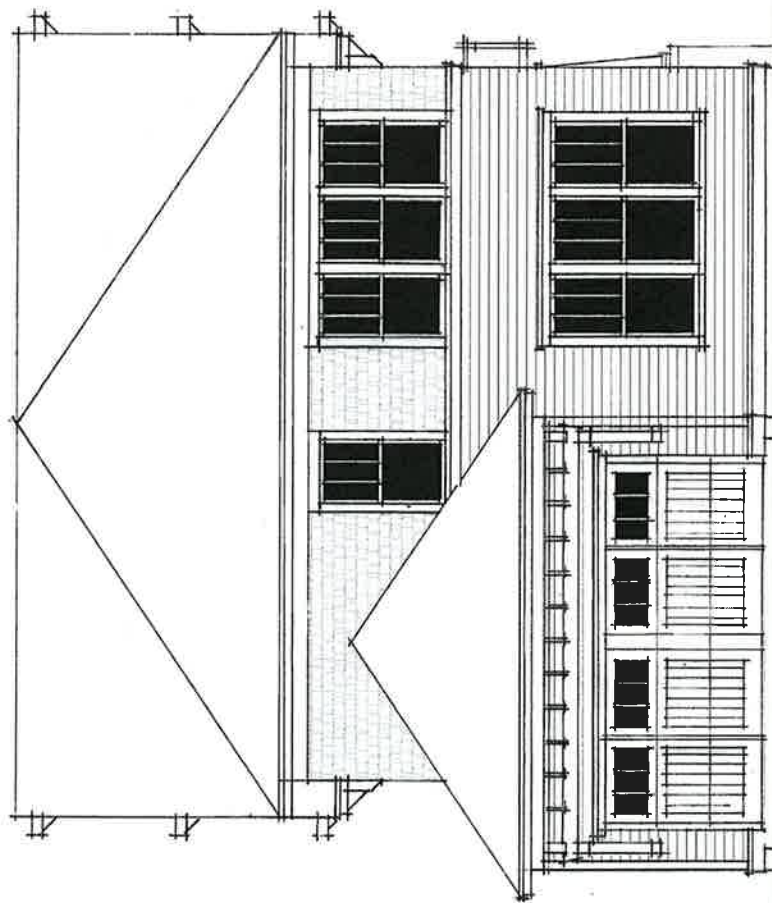
JUNE 16, 2016



ALTERNATE
FIRST FLOOR PLAN
ALTIORA PLAN
BLUE ASH, OHIO
JUNE 16, 2016



LEFT SIDE ELEVATION, "CRAFTSMAN"
ALTIRA PLAN
BLUE ASH, OHIO
M/I HOMES, INC.
JUNE 16, 2016



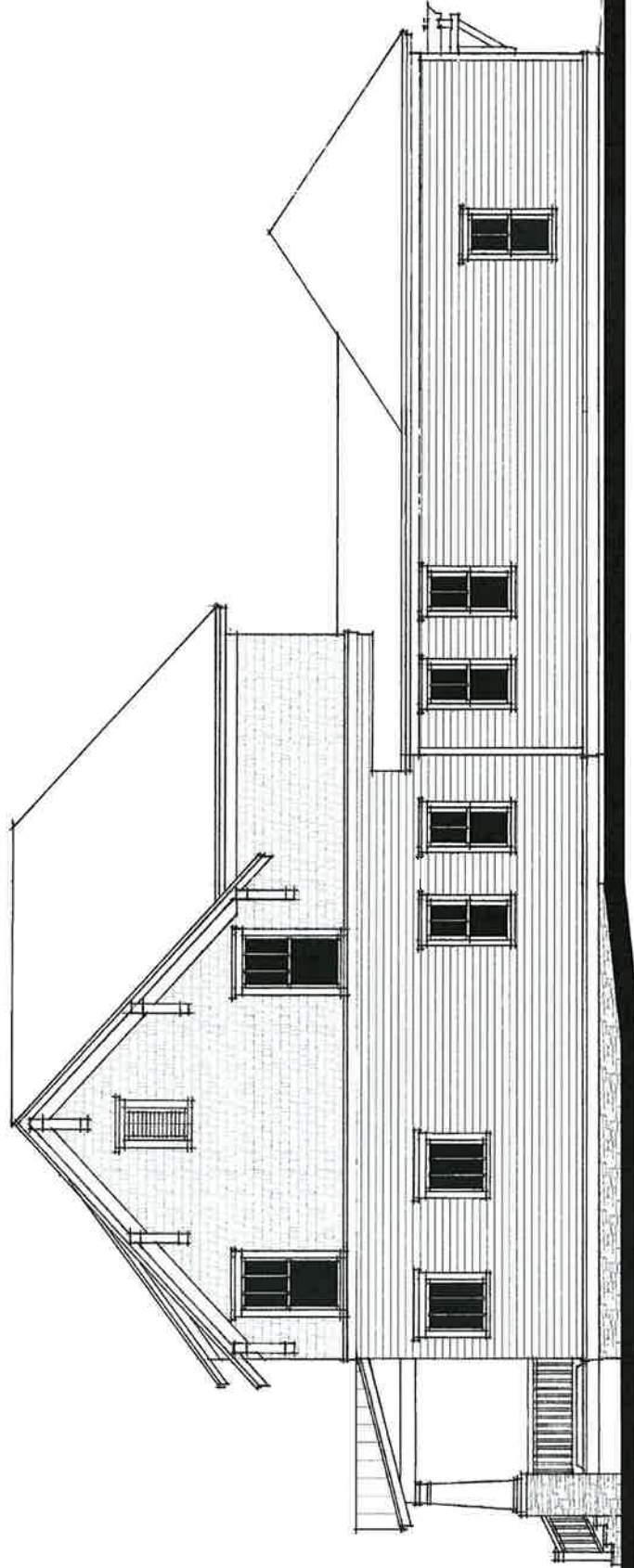
REAR ELEVATION, "CRAFTSMAN"

ALTIRA PLAN

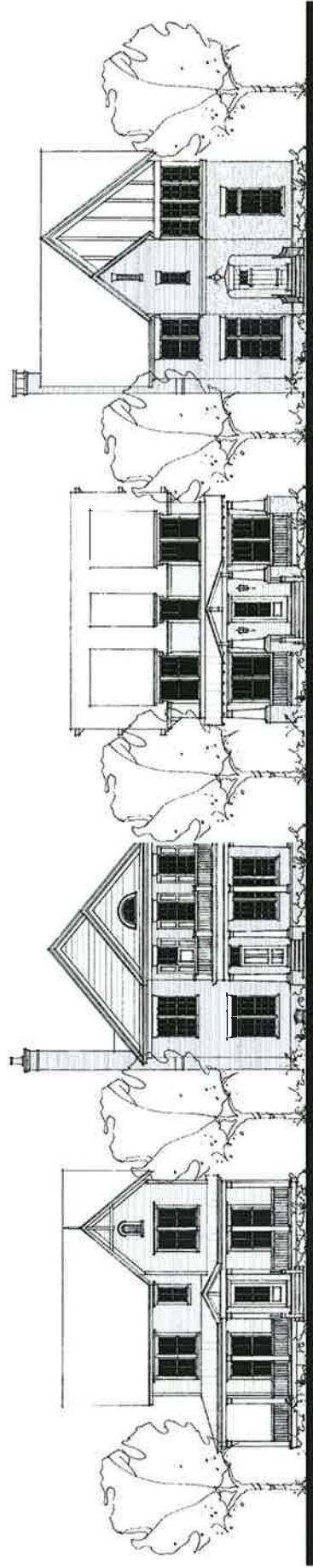
BLUE ASH, OHIO

M/I HOMES, INC.

JUNE 16, 2016



RIGHT SIDE ELEVATION, "CRAFTSMAN"
ALTORA PLAN
BLUE ASH, OHIO
M/I HOMES, INC.
JUNE 16, 2016



STREETSCAPE
BLUE ASH AIRPORT
BLUE ASH, OHIO
M/I HOMES, INC.
JUNE 16, 2016

Golf View - Illustrative Lot Plan

Golfview lots ensure that doors, windows, and porches greet the street as opposed to garages or large setbacks. This not only makes for a better streetscape in front but for an increased amount of private space in the rear yard. Porches will be a usable dimension (8 feet minimum depth) and often will wrap houses on corner lots.

Lots are typically 120 feet deep. They will be separated into nominally 46, 50, 55 and 60 foot widths. A minimum of an additional 10 feet will be added on corner lots. The 46's, 50's and 55's are alley loaded lots while the 60's are street loaded.

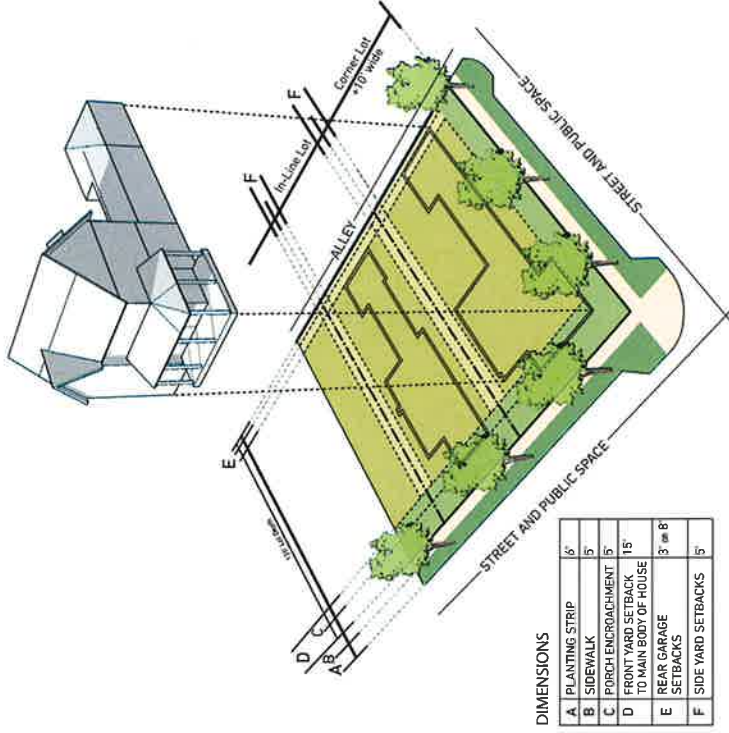
Typically, the front yard setback to the main mass of the house (the Main Body) will be 15 feet along street fronting elevations. Porches will be permitted to encroach into this setback a maximum of 5 feet. A 5-foot side yard separates houses from neighboring lots with a minimum 10 feet of separation between houses. There will be minimum use of lawn in front yards where xeriscape and gardens will predominate. Rear and side yards will contain a mix of soft and hardscape per the preferences of the buyer.

Garages will be at the rear of lots (except on street-loaded lots), either along alleys or accessed via a driveway around the house. The setback for garages will either be 3 feet or 8 feet. 8 feet allows a parallel guest parking space behind a garage. 3 feet is recommended on corner lots to neck down the apparent width of alley compared to streets. Where this does not happen, a minimum 4-foot tail wall should extend from the garage to the 3 foot setback line on corner lots. Attached and detached garages will be permitted.

Houses will be no taller than 2 and 1/2 stories atop a basement. Garages will either be one or two stories.



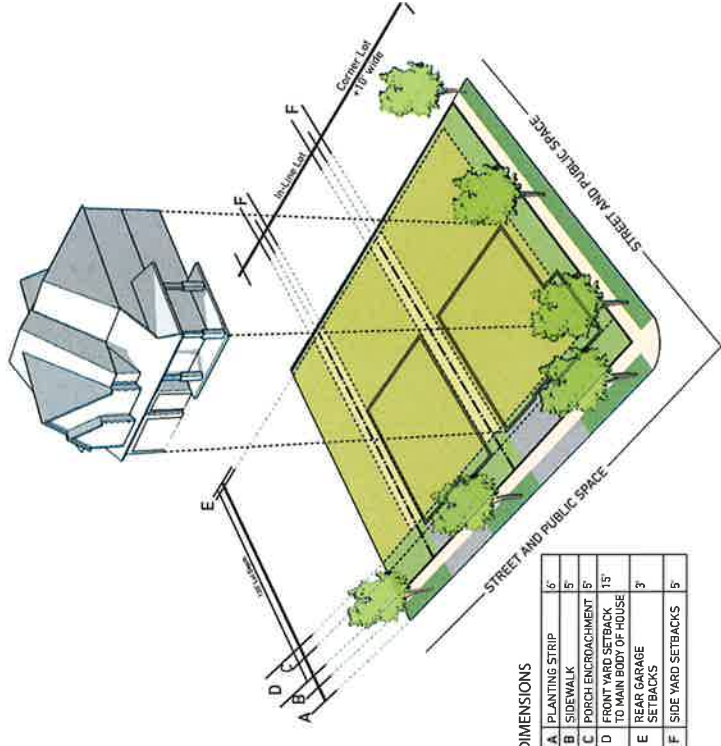
Westhaven - Franklin, TN



DIMENSIONS

A	PLANTING STRIP	6'
B	SIDEWALK	5'
C	PORCH ENCROACHMENT	5'
D	FRONT YARD SETBACK TO MAIN BODY OF HOUSE	15'
E	REAR GARAGE SETBACKS	3' OR 8'
F	SIDE YARD SETBACKS	5'

ALLEY-LOADED HOUSES



DIMENSIONS

A	PLANTING STRIP	6'
B	SIDEWALK	5'
C	PORCH ENCROACHMENT	5'
D	FRONT YARD SETBACK TO MAIN BODY OF HOUSE	15'
E	REAR GARAGE SETBACKS	3'
F	SIDE YARD SETBACKS	5'

STREET-LOADED HOUSES

